



# Syracuse City Planning Commission Meeting January 20, 2015

Begins at 6:00 p.m. in the City Council Chambers  
1979 West 1900 South, Syracuse, UT 84075

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## Joint Work Session with City Council

### **PLANNING COMMISSIONERS**

#### **CHAIR**

T.J. Jensen

#### **VICE CHAIR**

Ralph Vaughan

Curt McCuiston

Dale Rackham

Greg Day

Trevor Hatch

Troy Moultrie

1. **Meeting Called to Order**
  - Invocation or Thought
  - Pledge of Allegiance
  - Adoption of Meeting Agenda
2. **West Davis Corridor Alternative Solution**
3. **Syracuse City Master Transportation Plan**

## Planning Commission Regular Work Session

1. Department Business
2. Commissioner Reports
3. Upcoming Agenda Items
4. Discussion Items
  - a. Title X Code amendment pertaining to the fencing ordinance
  - b. Title X code amendments pertaining to the PRD, Planned Residential Development, Zone
  - c. Title X code amendments pertaining to the Architectural Review Committee and Design Standards
5. Adjourn



# Planning Commission Meeting

January 20, 2014

## *EXECUTIVE SUMMARY*

### **Item 2: Shared Solution Coalition Presentation**

Please refer to the following documents:

- Letter to the Mayor
- Transportation and Land Use Modeling Data

January 15, 2015

From: The Shared Solution Coalition

To: Mayor Terry Palmer, Syracuse City

RE: Shared Solution Alternative Land Use Scenario

### Background

For the last six months, UDOT, the Shared Solution Coalition and local communities have been collaboratively developing the Shared Solution alternative as part of the West Davis Corridor (WDC) study. This alternative is fundamentally different from all previously studied WDC alternatives because it proposes both transportation investments and a modified land use scenario in anticipation of future growth in West Davis and Weber counties.

The Shared Solution is an effort to realize the vision and principles of the Wasatch Choice for 2040 (WC2040). WC2040 is a publically vetted, proactive approach to growth on the Wasatch Front. While growth can be an opportunity, it also poses great challenges. Fortunately the WC2040 provides an actionable, nationally-recognized strategy to maintain our quality of life as we grow. The Wasatch Choice for 2040 prioritizes nine growth principles, including:

- Building and maintaining efficient infrastructure;
- Creating regional mobility through transportation choices;
- Developing healthy, safe communities;
- Providing housing choices for all ages and stages of life;
- Promoting a sense of community in our cities and towns.

To enact these principles, WC2040 encourages communities to:

- Focus growth in economic centers and along major transportation corridors;
- Create mixed-use centers;
- Target growth around transit stations;
- Encourage infill and redevelopment to revitalize declining parts of town; and
- Preserve working farms, recreational areas, and critical lands.

The Shared Solution alternative proposes implementing these principles and strategies in Davis and Weber Counties through a collaborative, integrated approach to transportation improvements and land use development.

### The Shared Solution Alternative

The West Davis Corridor Study is rooted in concerns about automobile congestion and delay in West Davis/Weber Counties in 2040. Like all other Study alternatives, the Shared Solution was modelled for its ability to reduce this anticipated automobile congestion and delay. In December 2014, the Shared Solution passed this Level 1 Screening, including significantly reduced congestion on east-west roadways. Passing Level 1 screening advanced the Shared Solution to Level 2 screening, where it will be evaluated for its impacts to the built and natural environments.

The success of the Shared Solution's transportation system depends on a proactive growth strategy. Again, learning from WC2040, the Shared Solution centers growth along major transportation

corridors, and brings better jobs/housing balance to Davis County, provides housing choices served by transit, and keeps open and agricultural lands for future generations. This land use vision was developed in collaboration with West Davis/Weber cities in a UDOT led workshop on September 4, 2014. In addition, this land use scenario, and corresponding employment and household distribution, was reviewed by the Wasatch Front Regional Council and deemed reasonable.

The Shared Solution's land use scenario envisions a variety of development types focused on major intersections and roadways. A number of arterials are transformed into boulevards, improving the functional and aesthetic quality of the road while maintaining existing Right-of-Way; building compact, mixed-use activity centers with a mix of jobs and housing at boulevard nodes; making transit a convenient, affordable choice; and improving safety for people choosing to walk or bike for transportation or recreation. In many cases, the Shared Solution reflects the visions of local communities. Many boulevards and activity centers are already planned town centers or redevelopment areas. The Shared Solution simply offers a regionally connected vision for local cities, supporting land use visions with transportation investments and recommending place making strategies like form-based code and aesthetic improvements.

While generally consistent with local plans, the Shared Solution does include some modification to existing municipal general plans in West Davis and Weber Counties. The Shared Solution Coalition is therefore asking all cities to review the Shared Solution land use scenario. We are asking cities to answer the following questions: if the roadway, transit, and active transportation elements of the Shared Solution alternative were to be implemented, does the city consider the 2040 land use scenario described in the attached documents to be reasonable (practical or feasible from a technical and economic standpoint)? And, would the city consider incorporating the land use scenario into its general plan or zoning map at the completion of UDOT's Environmental Impact Statement process if this alternative were ultimately selected?

Thank you for your consideration.

Sincerely,



Roger Borgenicht  
Co-Chair Utahns for Better Transportation for Shared Solution Coalition  
218 East 500 South  
Salt Lake City, UT 84111  
(801) 355-7085  
future@xmission.com

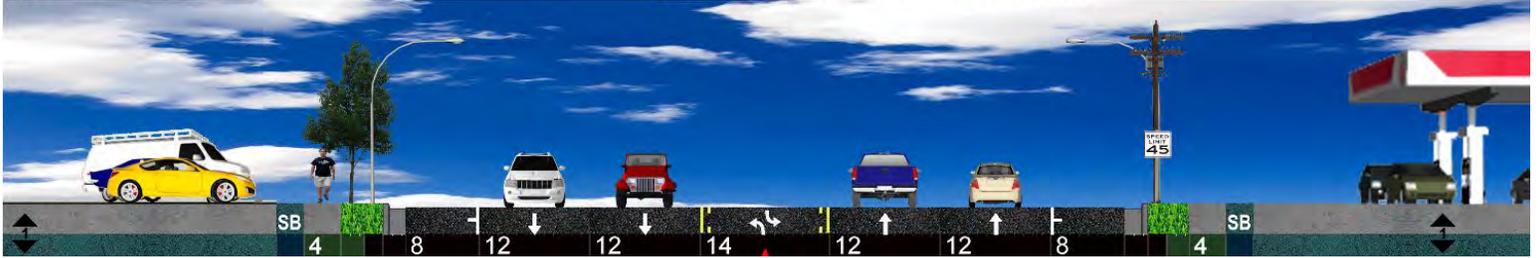


# Sample Boulevard Roadway Sections

Typical Existing Arterial Conditions

Current Width:100' (100' ROW)

Curb to Curb:83'



Center median roadway design near existing residential uses

Current Width:100' (100' ROW)

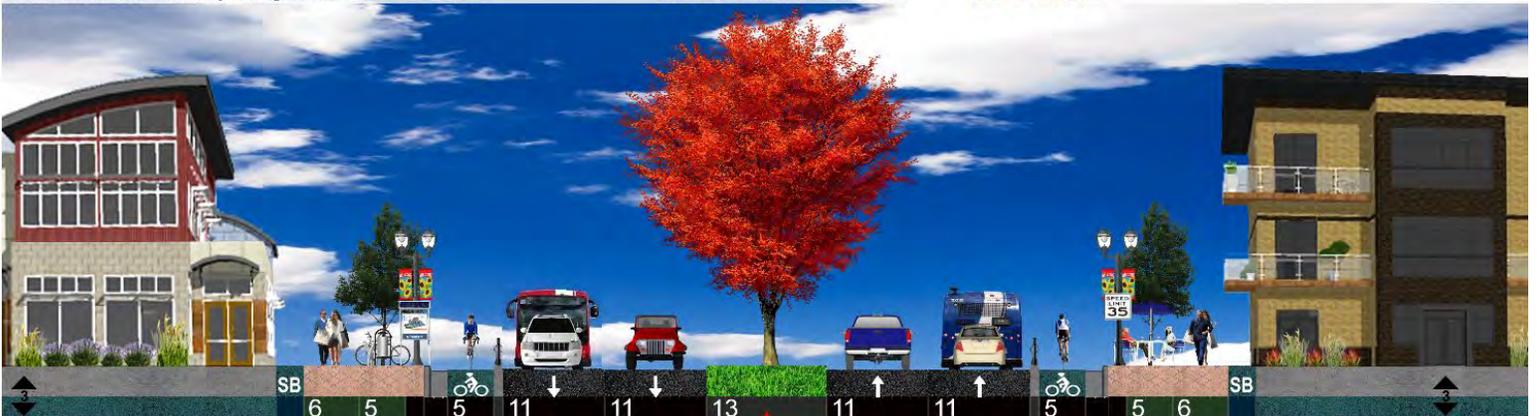
Curb to Curb:78'



Center median roadway design with new mixed-uses

Current Width:100' (100' ROW)

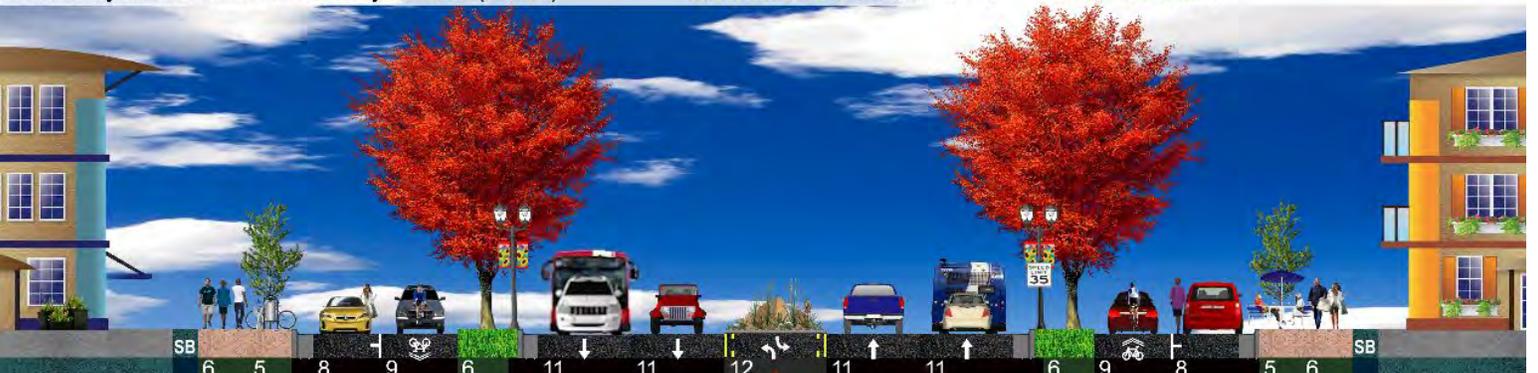
Curb to Curb:74'



Multi-Way Boulevard within activity centers (nodes)

Current Width:136' (136' ROW)

Curb to Curb:61'



Boulevards can often be designed without additional right of way. Speed limits at nodes would be slower, but travel time will often be faster due to less congestion. Sometimes land uses will redevelop, but often they will stay the same – especially near established single-family neighborhoods. Where practical and desirable, right-of-way could expand to include on-street parking and better protection of bikes and pedestrians from traffic. Shoulders can often be used by buses at peak hours.

\*Landscaping improvements are typically funded and maintained by the local communities.

**Shared Solutions Alternative (12/12/14)**  
**West Davis Corridor EIS**

Description	Daily Total Delay (Hr)	North-South Road Lane-Miles with PM Period V/C >= 0.9	East-West Road Lane-Miles with PM Period V/C >= 0.9	Vehicle Miles Traveled (VMT) with PM Period V/C >= 0.9	Vehicle Hours Traveled (VHT) with PM Period V/C >=0.9
<b>NO ACTION</b>	10,760	43.5	26.9	245,500	9,490
<b>MEAN</b>	8,950	31.4	23.2	177,700	7,160
<b>1st QUARTILE</b>	8,060	17.9	20.2	97,400	5,340

Alt.	Facility Type	Description					
SS	Shared Solutions	The Shared Solutions alternative	8,750.0	18.4	10.5	68,800	3,760

# Shared Solution Alternative

## Land Use Modeling Assumptions and Methodology

January 14, 2015



WEST DAVIS  
CORRIDOR

This is a summary of the assumptions and methodology used in developing the land use data inputs to the WFRC travel model for analyzing the Shared Solution Alternative. These have been collaboratively developed through multiple meetings with the Shared Solution Coalition and the WDC study team. It is important to realize that the resulting data is simply an estimate of what land use might look like if the mixed use principles espoused by the Shared Solution Alternative are implemented by local governments. The details of which parcels will redevelop and the density to which they will redevelop are all best guesses. Reality will obviously vary.

### 1. Modeling Constraints

- a. Residential and commercial categories will remain consistent with county-wide control totals (i.e. land use growth can be moved throughout the county, but not added or subtracted from the total)
- b. The resulting study area trip generation in the WFRC travel model will be approximately equal to that of the other West Davis Corridor alternatives

### 2. Redevelopment Parcel Identification

- a. Based on mixed use developments in other areas, it was assumed that:
  - i. boulevards and Main Street communities would have a total width of 500 feet (250 feet on either side of the roadway centerline)
  - ii. town centers would comprise a square  $\frac{1}{4}$  mile in length on each side (centered on the key intersection)
  - iii. redevelopment would occur within a 750 foot radius of key transit stops in Layton (assumed to be town centers)
- b. Parcels were selected for potential redevelopment using ET+ data based on the following criteria:
  - i. agricultural and vacant land uses
  - ii. retail land uses with structures built prior to 2009
  - iii. office and industrial land uses with structures built prior to 1989
  - iv. single family land uses with a lot size greater than 1 acre and mobile home land uses
- c. Parcels were generally clipped at the boulevard or town center boundary; however, there were locations along SR-126 and in Layton around I-15 where the entire parcel was selected
- d. Approximately  $\frac{1}{2}$  of the parcels within the buffer areas (1,780 acres out of 3,653 acres) were selected as candidates for redevelopment

### 3. Redevelopment Mixed Use and Density Estimation

- a. Boulevard and town center locations and intensities were based on city inputs from the Shared Solution land use workshop
- b. The range of floor area ratios (FAR) and residential densities from the Wasatch Choices for 2040 was used as a starting point
- c. The boulevard and town center development types were further subdivided such that development intensity generally increased from west to east (i.e. the closer to I-15 the higher the density)
- d. To improve the jobs / housing balance in the study area approximately 11,000 additional jobs were moved into the study area and about 1,500 houses were moved out

- e. It was assumed that 1/3 of the household growth and 80% of the employment growth in the study area would take place within the mixed use development / redevelopment areas
- f. Household and employment growth were distributed among the various boulevards, town centers, etc. based on the target FAR for each development type (average household size and household income were also estimated for each development type, which, on average, were each assumed to be less than the original overall study area average)
- g. Travel model TAZs were split to match the mixed use development / redevelopment areas and the household and employment growth were distributed among the TAZs based on the proportion of each development type within each TAZ (adjustments were made to account for existing land uses that would be redeveloped)

#### **4. Adjustments to Non-Redevelopment Areas**

- a. Growth outside of the mixed use development / redevelopment zones, but inside the study area was distributed through those zones based on the original 2009 to 2040 growth assumptions and an adjustment factor that placed more growth on the east side of the study area than on the west side
- b. Outside of the study area, land use adjustments were made to account for households that were moved out of the study area and jobs that were moved into the study area
  - i. new households were assumed to be added to Ogden and south Davis County so as to be closer to employment centers
  - ii. employment growth was taken most heavily from the fringes of Weber and Davis Counties and less heavily from the more urbanized areas



# WEST DAVIS CORRIDOR

ENVIRONMENTAL IMPACT STATEMENT

## Legend

— FrontRunner

### Station Communities\*

- SC-1B | 14 Units
- SC-2C | 29 Units
- SC-3B | 35 Units

### Town Centers\*

- TC-1A | 8 Units
- TC-1B | 11 Units
- TC-1C | 14 Units
- TC-2A | 16 Units
- TC-2B | 18 Units
- TC-2C | 21 Units
- TC-3B | 26 Units
- TC-3C | 28 Units

### Boulevard Communities\*

- BC-1A | 6 Units
- BC-1B | 8 Units
- BC-1C | 9 Units
- BC-2B | 12 Units
- BC-2C | 14 Units
- BC-3B | 15 Units

### Main Street\*

- MS-1A | 8 Units

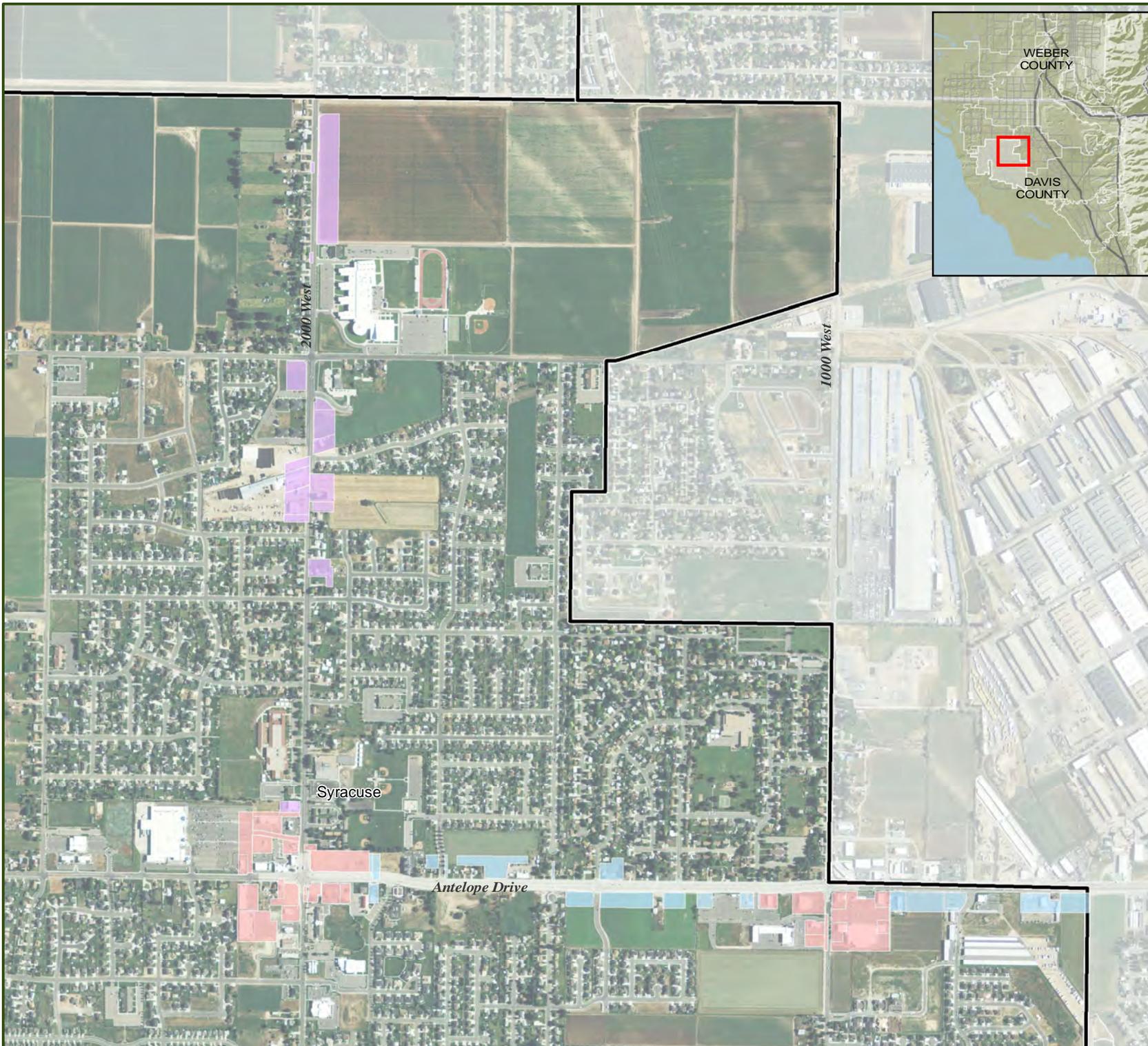
□ City Boundary

\* Units are households per acre of residential land use.



**Proposed Shared  
Solution  
Redevelopment  
Areas – Syracuse**

**Figure 1**



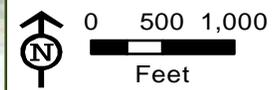
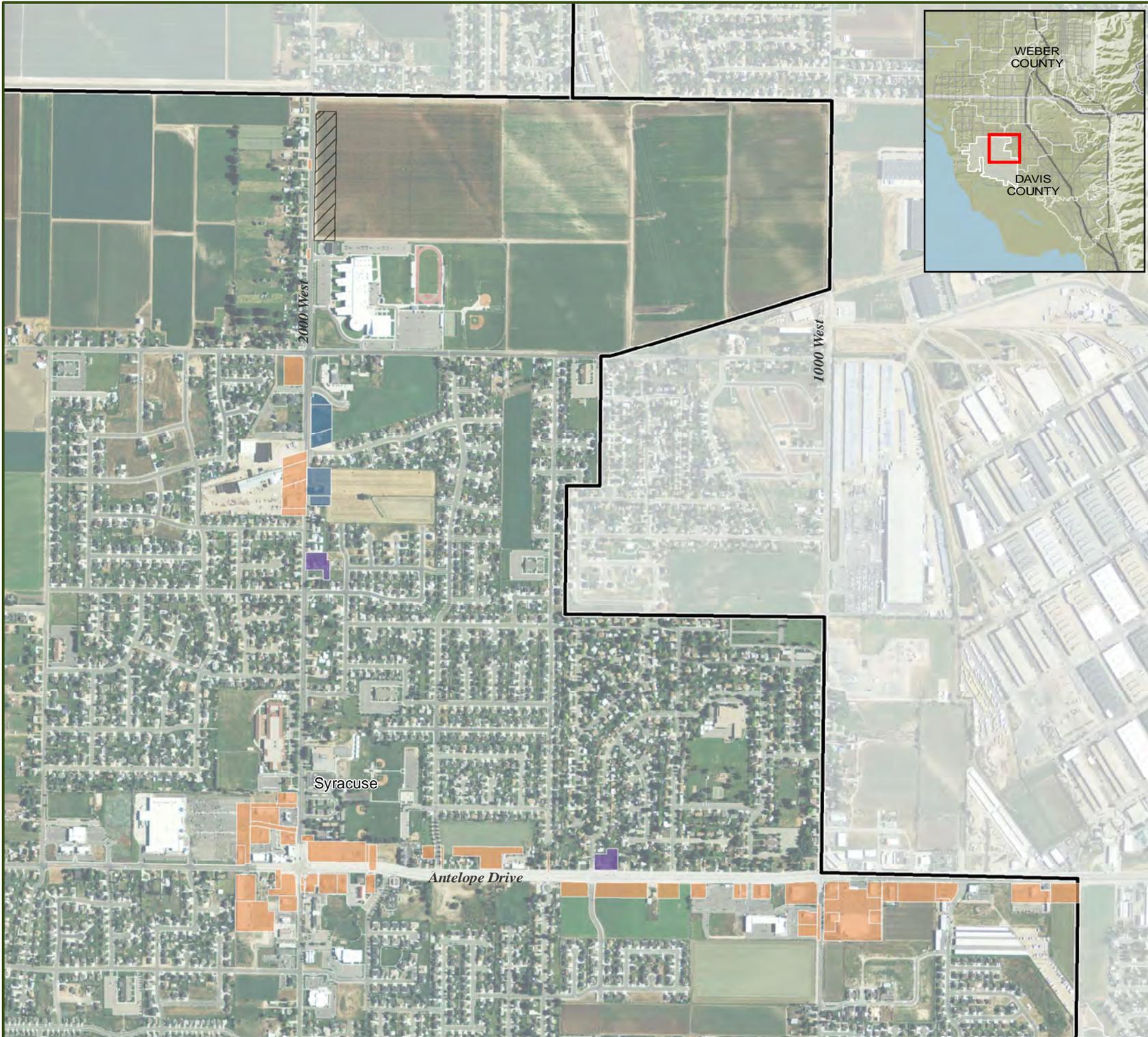
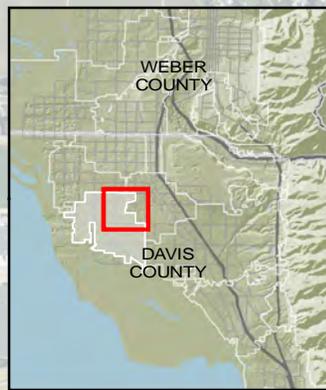


# WEST DAVIS CORRIDOR

ENVIRONMENTAL IMPACT STATEMENT

## Legend

- FrontRunner
- City Planned Land Use**
  - General Commercial
  - Neighborhood Services
  - Professional Office
  - Unclassified
- City Boundary



**Planned Land Use  
Syracuse**

**Figure 2**

Syracuse

Shared Solution Alternative Proposed Land Use	Syracuse Future Land Use	Acres	Residential Data				Commercial Data				Floor Area Ratio (FAR)	Number of Floors
			Residential Percentage	Residential Acreage	Households per Acre of Residential Land Use	Shared Solution Proposed Households	Commercial Percentage	Commercial Acreage	Shared Solution Proposed Retail Employment	Shared Solution Proposed Office Employment		
BC-1B	General Commercial	15.2	69%	10.5	8	84	31%	4.7	55	74	0.3	1.2
	Neighborhood Services	1.1	69%	0.7	8	6	31%	0.3	4	5	0.3	1.2
<b>Total</b>		<b>16.2</b>		<b>11.2</b>		<b>89</b>		<b>5.0</b>	<b>58</b>	<b>79</b>		
MS-1A	General Commercial	5.7	50%	2.8	8	23	50%	2.8	29	59	0.32	1.2
	Neighborhood Services	1.1	50%	0.5	8	4	50%	0.5	6	11	0.32	1.2
	Professional Office	4.6	50%	2.3	8	18	50%	2.3	23	48	0.32	1.2
	Unclassified	6.4	50%	3.2	8	26	50%	3.2	33	66	0.32	1.2
<b>Total</b>		<b>17.7</b>		<b>8.8</b>		<b>71</b>		<b>8.8</b>	<b>90</b>	<b>184</b>		
TC-1B	General Commercial	28.0	53%	14.8	11	163	47%	13.2	160	361	0.4	1.7
<b>Total</b>		<b>28.0</b>		<b>14.8</b>		<b>163</b>		<b>13.2</b>	<b>160</b>	<b>361</b>		
<b>Total for all categories</b>		<b>61.9</b>	<b>56%</b>	<b>34.9</b>	<b>9</b>	<b>323</b>	<b>44%</b>	<b>27.0</b>	<b>308</b>	<b>624</b>		

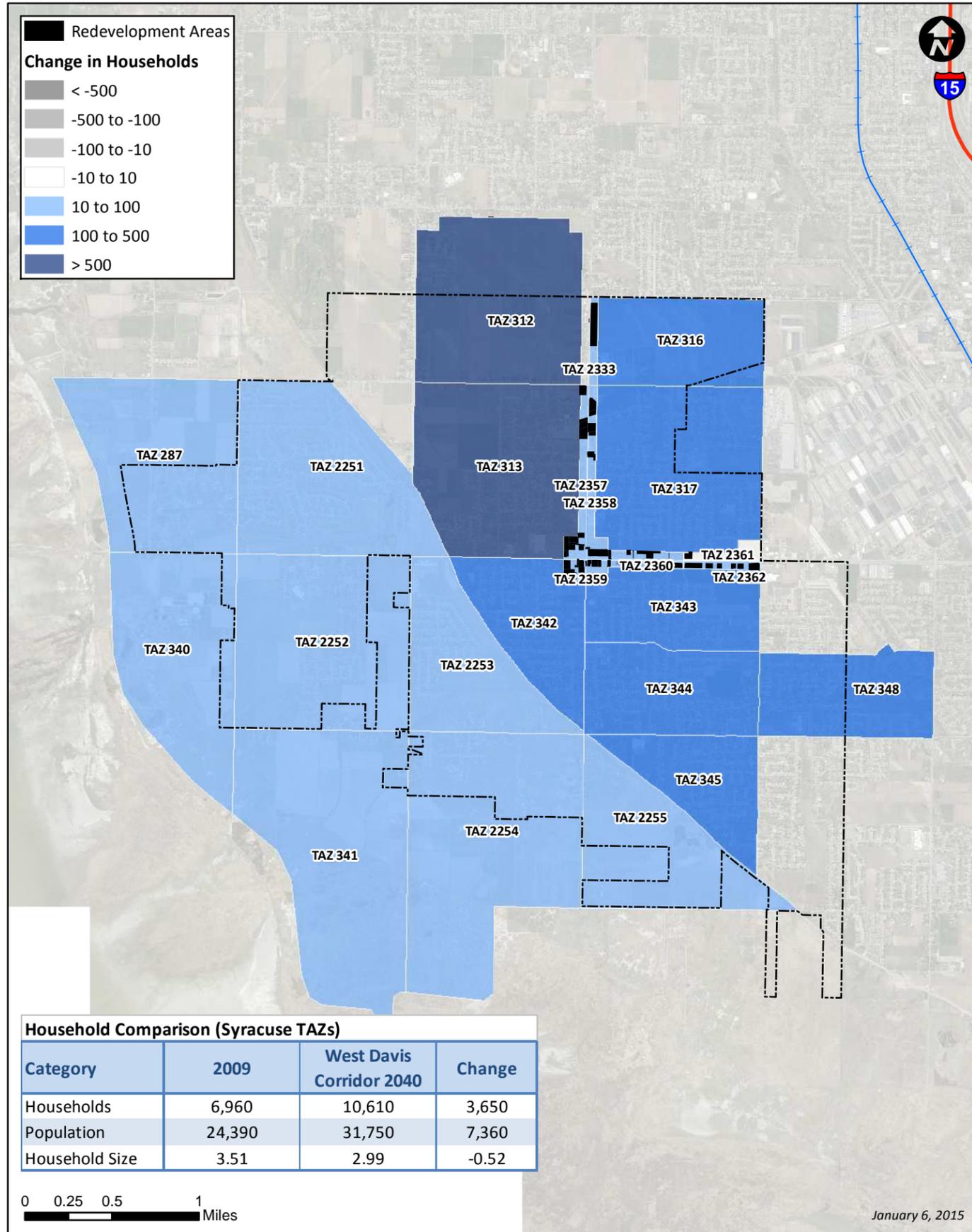
## Land Use Designations

Code	Zoning Designation	Floor Area Ratio (average)	Households per Acre of Residential Land Use	Average Number of Building Floors	
TC	<b>Town Center</b> 	<i>Town centers provide localized services of tens of thousands of people within a two to three mile radius. One- to three- story buildings for employment and housing are characteristic. Town centers have a strong sense of community identity and are well served by transit.</i>			
	TC-1A	Low Density	0.31	8 units/acre	1.7
	TC-1B		0.40	11 units/acre	1.7
	TC-1C		0.36	14 units/acre	2.0
	TC-2A	Medium Density	0.59	16 units/acre	2.3
	TC-2B		0.67	18 units/acre	2.6
	TC-2C		0.76	21 units/acre	2.9
	TC-3B	High Density	0.95	26 units/acre	3.4
	TC-3C		1.04	28 units/acre	3.7
	SC	<b>Station Community</b> 	<i>Station Communities are geographically small, high-intensity centers surrounding high capacity transit stations, Each helps pedestrians and bicyclists access transit without a car. Station Communities vary in their land use: some feature employment, others focus on housing, and may include a variety of shops and services.</i>		
SC-1B		Low Density	0.50	14 units/acre	2.0
SC-2C		Medium Density	1.05	29 units/acre	3.3
SC-3B		High Density	1.30	35 units/acre	4.5

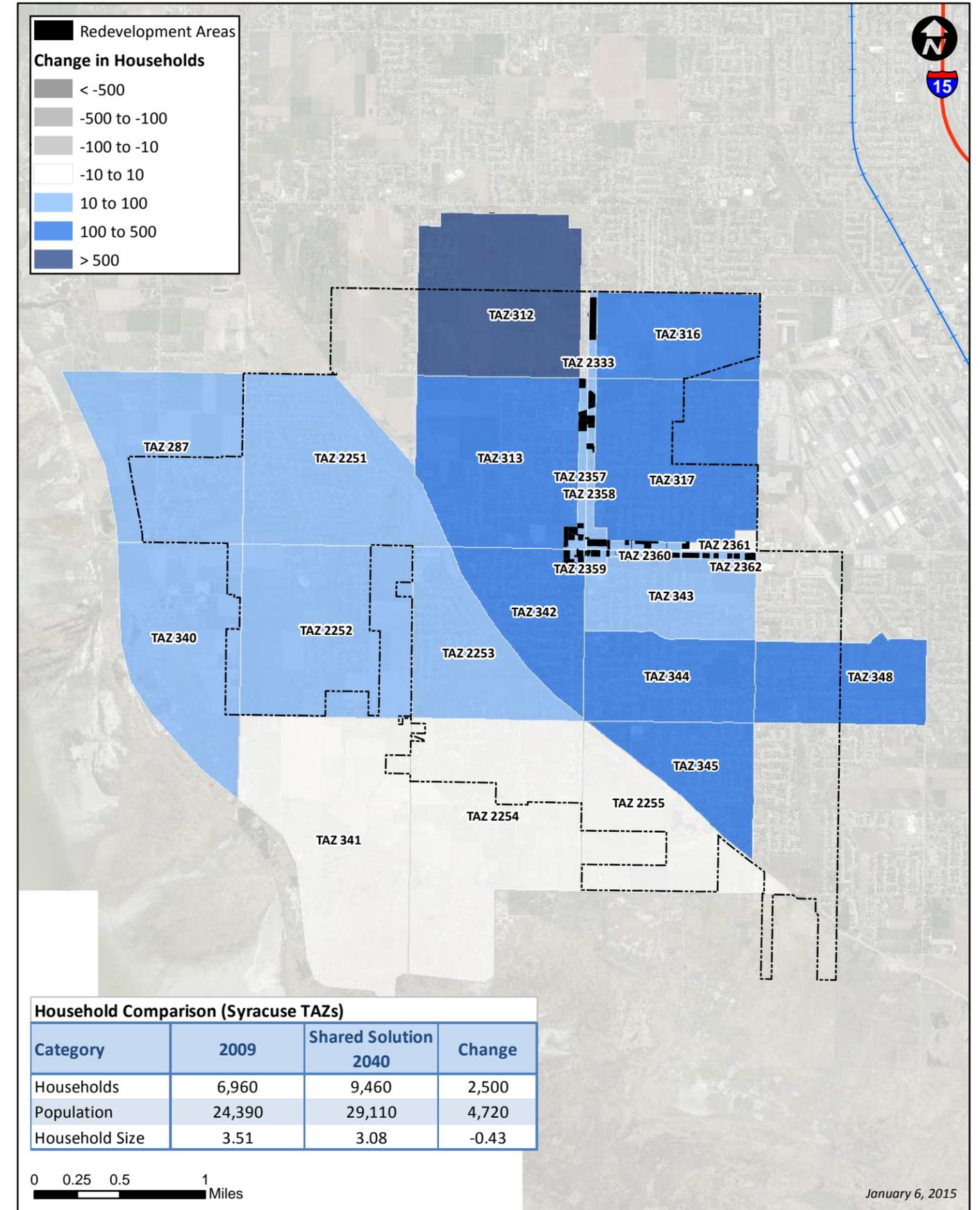
## Land Use Designations

Code	Zoning Designation	Floor Area Ratio (min/max)	Households per Acre of Residential Land Use	Average Number of Building Floors	
<b>BC</b>	<b>Boulevard Community</b>  	<p><i>A Boulevard Community is a linear center couple with a transit route. Unlike a Main Street, a Boulevard Community may not necessary have a commercial identity, but may vary between housing, employment, and retail along any given stretch. Boulevard Communities create positive sense of place for adjacent neighborhoods by ensuring that walking and bicycling are safe and comfortable even as traffic flows are maintained.</i></p>			
	BC-1A	Low Density	0.23	6 units/acre	1.0
	BC-1B		0.30	8 units/acre	1.2
	BC-1C		0.36	9 units/acre	1.4
	BC-2B	Medium Density	0.45	12 units/acre	1.8
	BC-2C		0.53	14 units/acre	1.9
	BC-3B	High Density	0.54	15 units/acre	2.0
<b>MS</b>	<b>Main Street Community</b>  	<p><i>Main Streets are a linear town center. Each has a traditional commercial identity but are on a community scale with a strong sense of the immediate neighborhood. Main streets prioritize pedestrian-friendly features, but also benefit from good auto-access and often transit.</i></p>			
	MS-1A	Low Density	0.32	8 units/acre	1.2

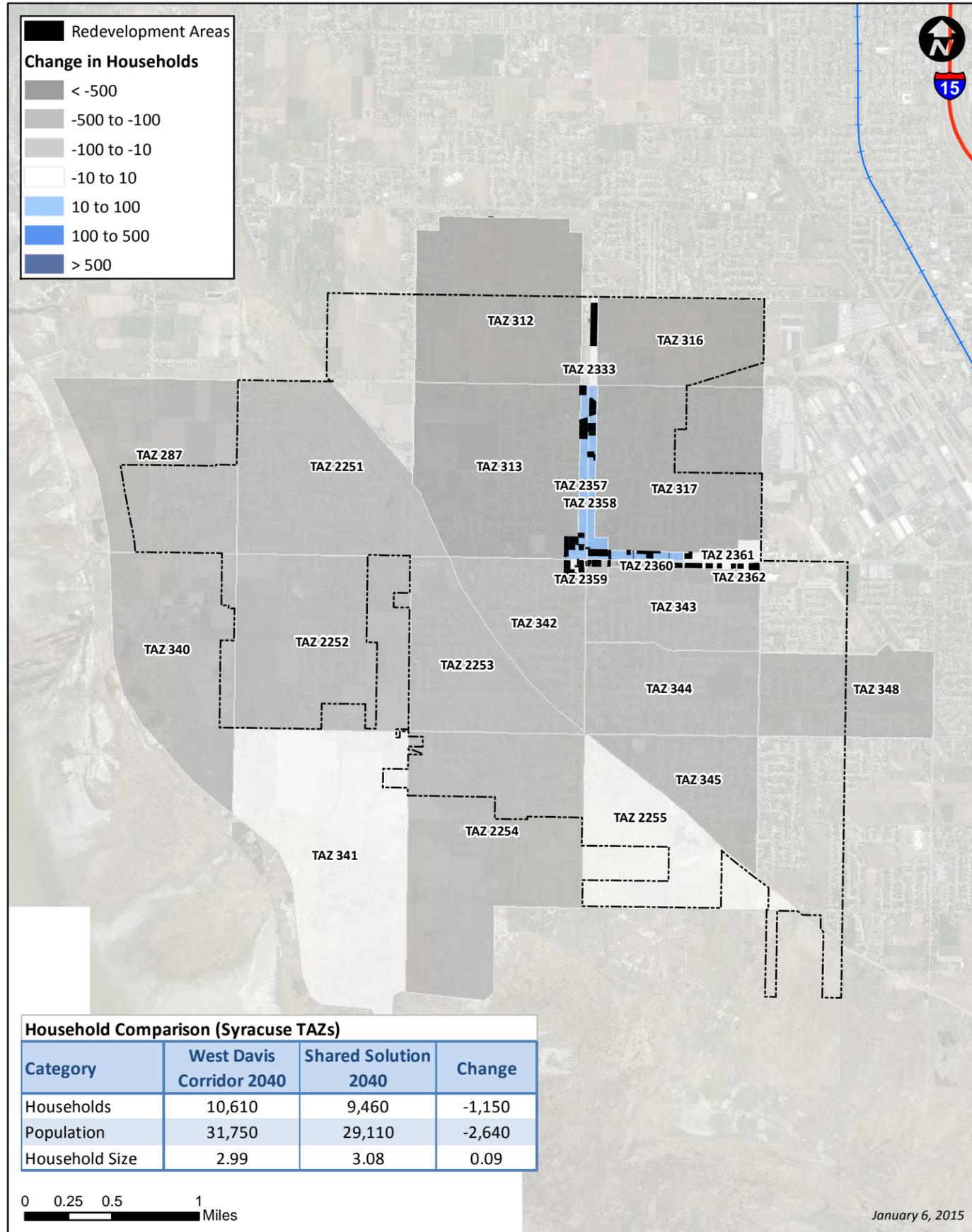
## Syracuse Change in Households (2009 to West Davis Corridor 2040)



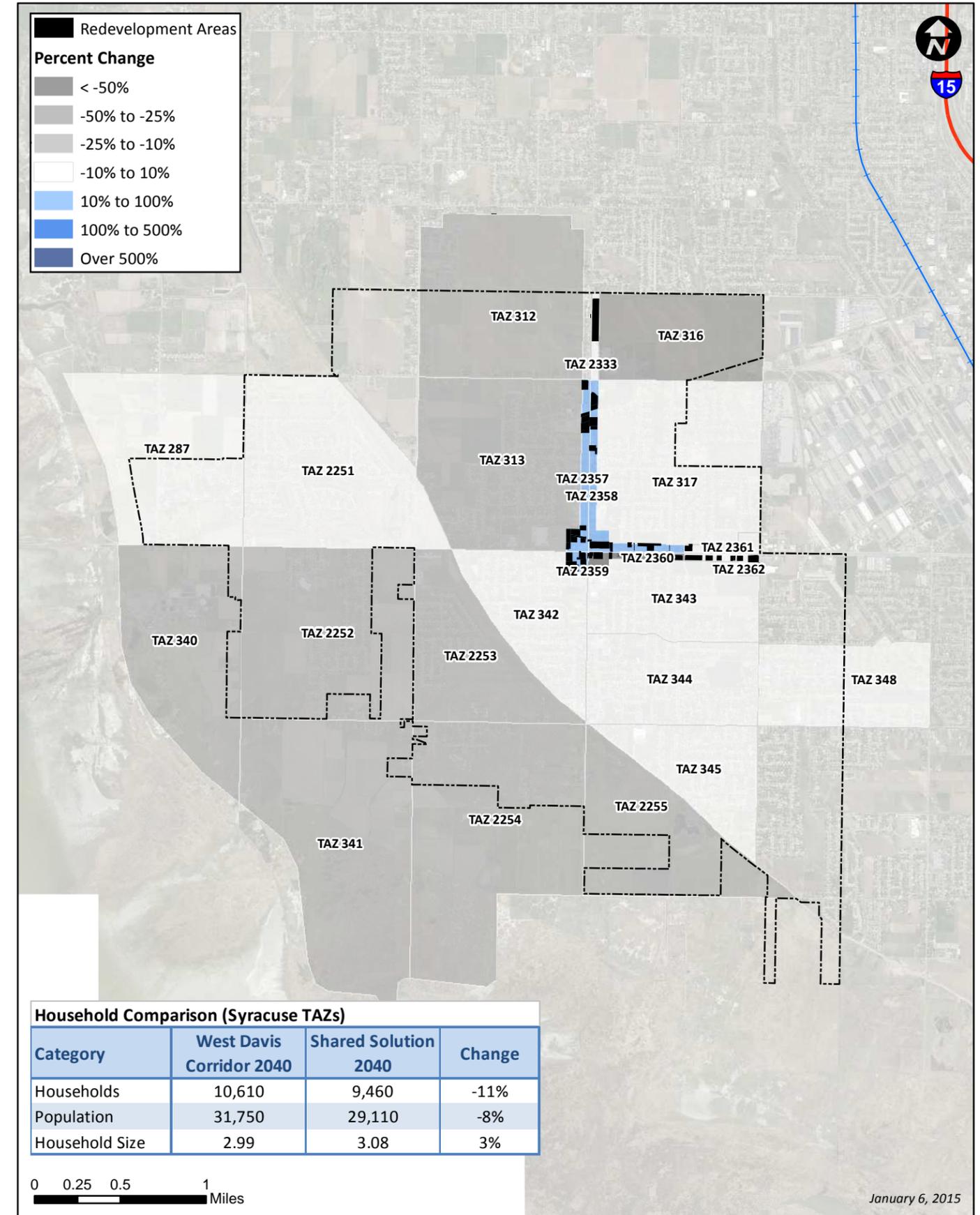
## Syracuse Change in Households (2009 to Shared Solution 2040)



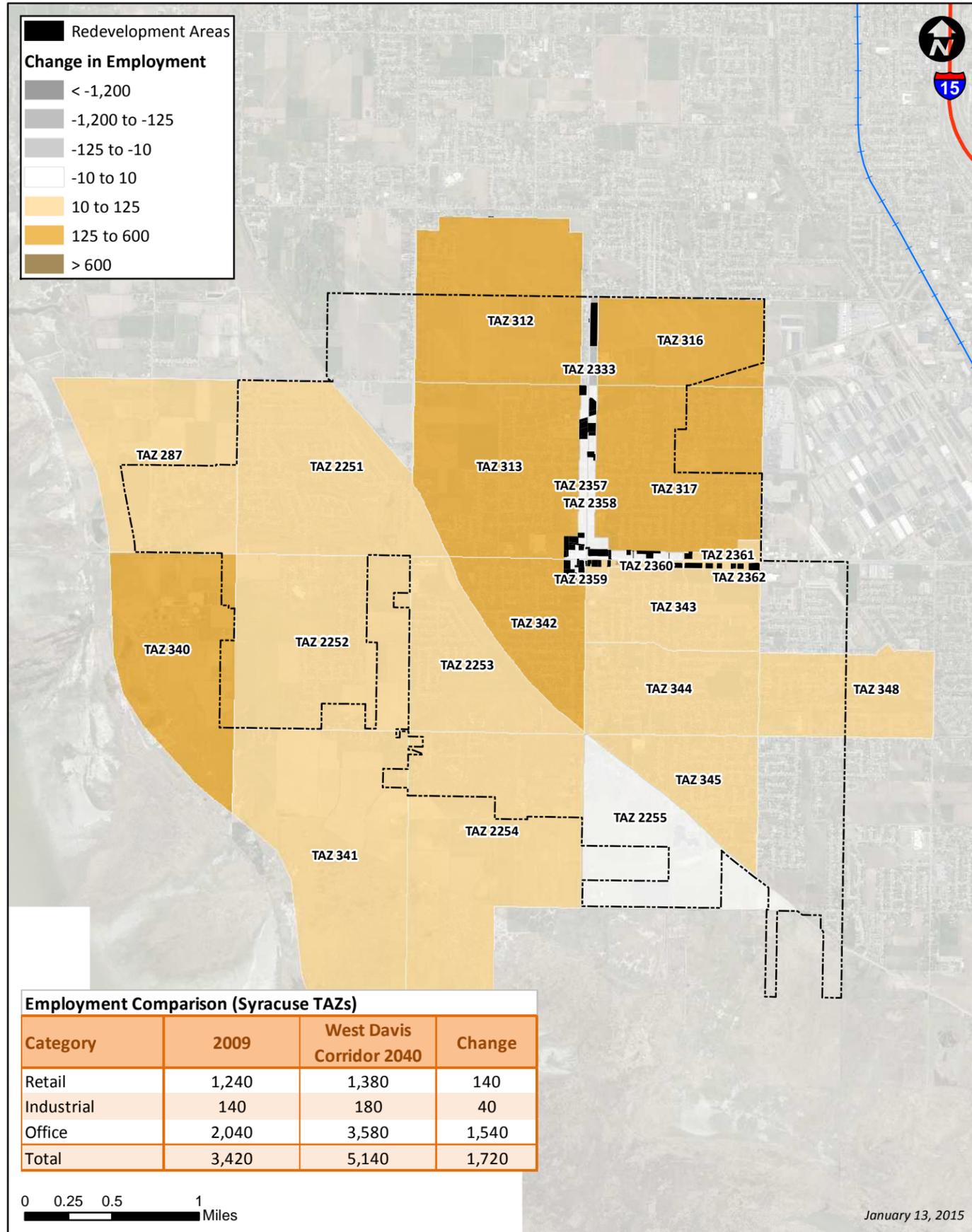
## Syracuse Change in Households (West Davis Corridor 2040 to Shared Solution 2040)



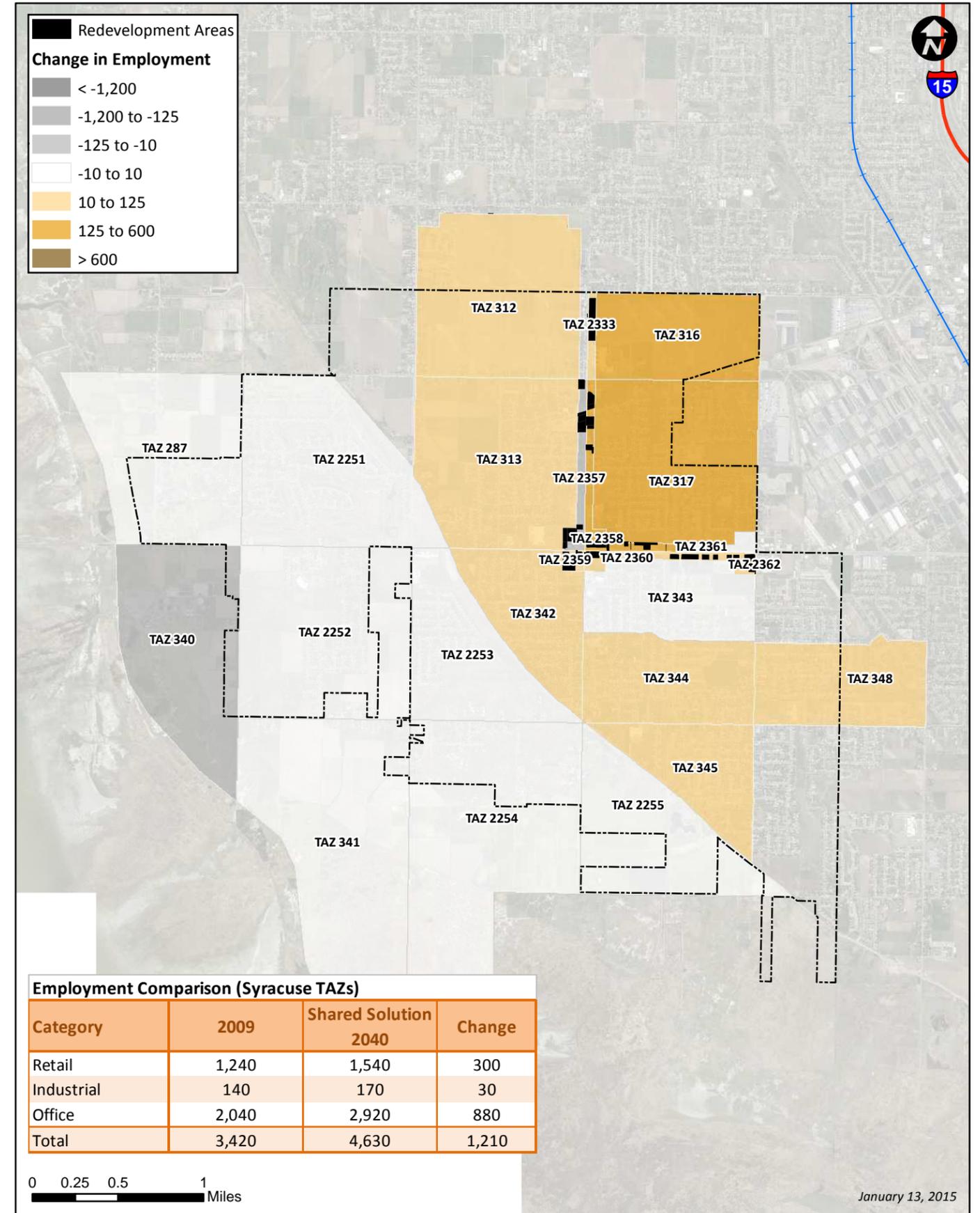
## Syracuse % Change in Households (West Davis Corridor 2040 to Shared Solution 2040)



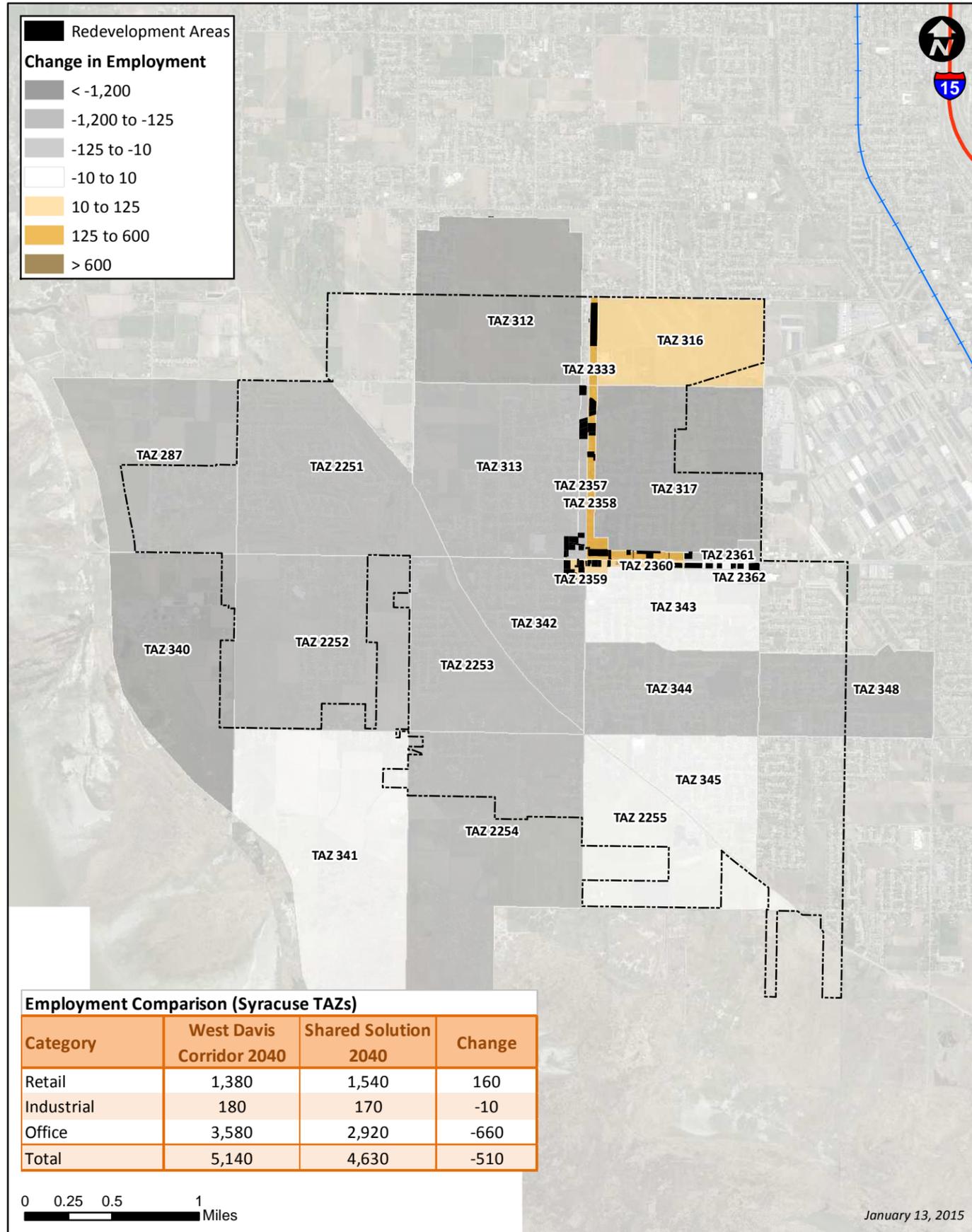
## Syracuse Change in Employment (2009 to West Davis Corridor 2040)



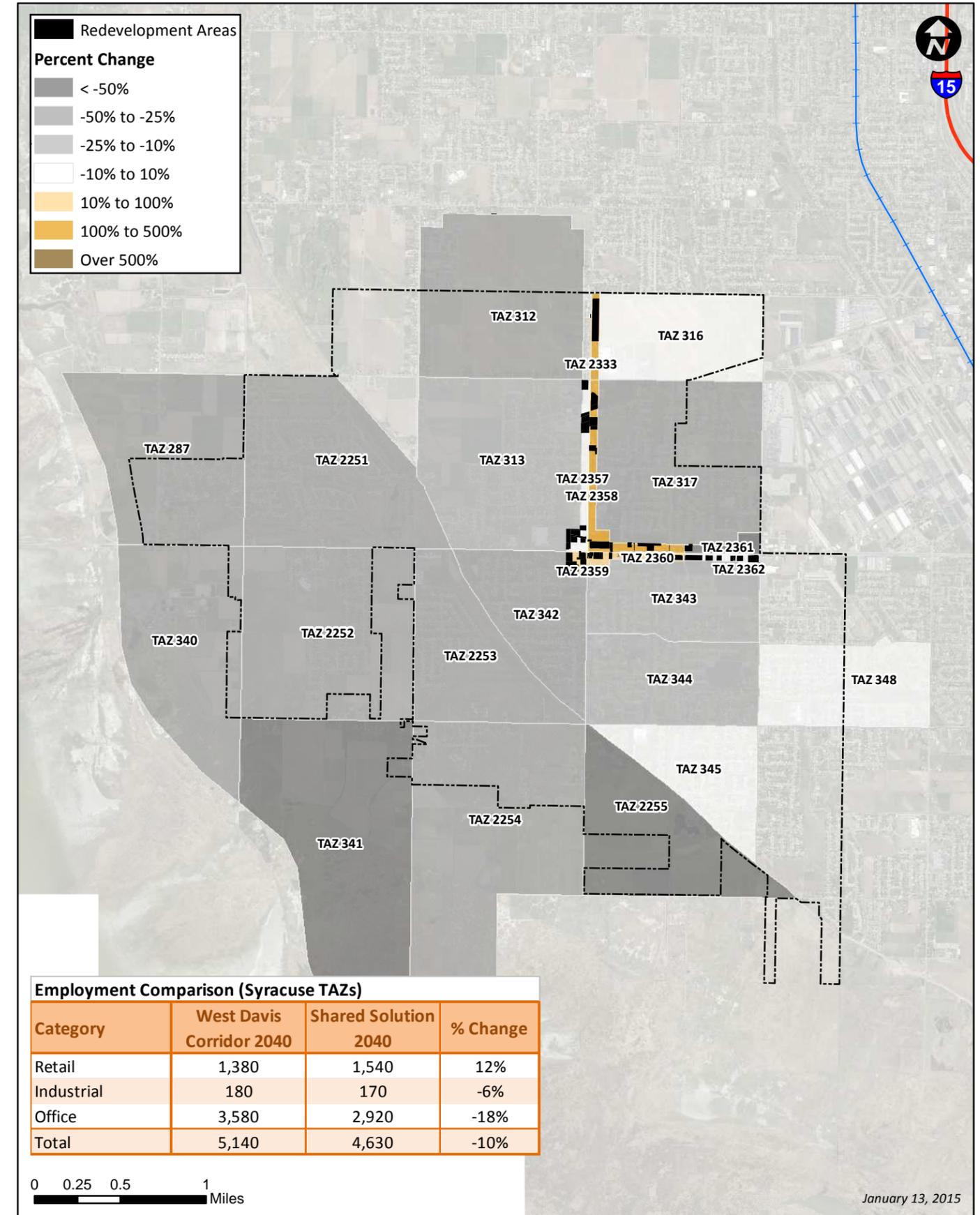
## Syracuse Change in Employment (2009 to Shared Solution 2040)



## Syracuse Change in Employment (West Davis Corridor 2040 to Shared Solution 2040)



## Syracuse % Change in Employment (West Davis Corridor 2040 to Shared Solution 2040)



### Syracuse TAZ Household and Population Data

TAZ	Households					Population					Household Size				
	2009	West Davis Corridor 2040	Shared Solution 2040	Change WDC 2040 to SS 2040	% Change WDC 2040 to SS 2040	2009	West Davis Corridor 2040	Shared Solution 2040	Change WDC 2040 to SS 2040	% Change WDC 2040 to SS 2040	2009	West Davis Corridor 2040	Shared Solution 2040	Change WDC 2040 to SS 2040	% Change WDC 2040 to SS 2040
287	282	335	302	-33	-10%	1,035	1,070	1,005	-65	-6%	3.67	3.19	3.33	0.14	4%
312	206	973	732	-241	-25%	705	2,704	2,074	-630	-23%	3.42	2.78	2.83	0.05	2%
313	590	1,106	944	-162	-15%	2,191	3,513	3,162	-351	-10%	3.71	3.18	3.35	0.17	5%
316	79	506	372	-134	-26%	265	1,288	849	-439	-34%	3.35	2.55	2.28	-0.27	-10%
317	1,085	1,584	1,427	-157	-10%	3,643	4,603	4,315	-288	-6%	3.36	2.91	3.02	0.11	4%
340	198	249	217	-32	-13%	717	779	715	-64	-8%	3.62	3.13	3.29	0.16	5%
341	39	54	45	-9	-17%	135	159	139	-20	-13%	3.46	2.94	3.12	0.18	6%
342	645	823	767	-56	-7%	2,061	2,279	2,181	-98	-4%	3.20	2.77	2.84	0.07	3%
343	489	592	560	-32	-5%	1,685	1,767	1,705	-62	-4%	3.45	2.98	3.05	0.07	2%
344	759	990	918	-72	-7%	2,619	2,963	2,827	-136	-5%	3.45	2.99	3.08	0.09	3%
345	514	729	662	-67	-9%	1,785	2,185	2,055	-130	-6%	3.47	3.00	3.11	0.11	4%
348	791	941	917	-24	-3%	2,971	3,080	3,033	-47	-2%	3.76	3.27	3.31	0.04	1%
2251	485	576	519	-57	-10%	1,780	1,839	1,727	-112	-6%	3.67	3.19	3.33	0.14	4%
2252	314	392	343	-49	-12%	1,129	1,229	1,137	-92	-7%	3.60	3.14	3.31	0.17	6%
2253	197	247	216	-31	-13%	709	771	711	-60	-8%	3.60	3.13	3.30	0.17	5%
2254	46	63	52	-11	-17%	158	188	168	-20	-10%	3.43	2.98	3.22	0.24	8%
2255	32	43	36	-7	-16%	106	126	115	-11	-9%	3.31	2.93	3.19	0.26	9%
2333	-	25	24	-1	-2%	-	64	62	-2	-3%	3.35	2.56	2.54	-0.02	-1%
2357	41	71	93	22	32%	152	225	265	40	18%	3.71	3.17	2.84	-0.33	-10%
2358	102	139	154	15	11%	343	404	447	43	11%	3.36	2.91	2.90	-0.01	-0%
2359	-	28	34	6	20%	-	78	76	-2	-2%	3.20	2.79	2.26	-0.53	-19%
2360	2	47	20	-27	-57%	7	140	51	-89	-63%	3.50	2.98	2.52	-0.46	-15%
2361	59	59	60	1	1%	198	172	185	13	8%	3.36	2.92	3.10	0.18	6%
2362	-	42	42	-0	-1%	-	125	103	-22	-17%	3.45	2.98	2.48	-0.50	-17%
<b>Total</b>	<b>6,955</b>	<b>10,614</b>	<b>9,456</b>	<b>-1,158</b>	<b>-11%</b>	<b>24,394</b>	<b>31,751</b>	<b>29,106</b>	<b>-2,645</b>	<b>-8%</b>	<b>3.51</b>	<b>2.99</b>	<b>3.08</b>	<b>0.09</b>	<b>3%</b>

### Syracuse TAZ Employment Data

TAZ	Total Employment					Retail Employment					Industrial Employment					Office Employment				
	2009	West Davis Corridor 2040	Shared Solution 2040	Change WDC 2040 to SS 2040	% Change WDC 2040 to SS 2040	2009	West Davis Corridor 2040	Shared Solution 2040	Change WDC 2040 to SS 2040	% Change WDC 2040 to SS 2040	2009	West Davis Corridor 2040	Shared Solution 2040	Change WDC 2040 to SS 2040	% Change WDC 2040 to SS 2040	2009	West Davis Corridor 2040	Shared Solution 2040	Change WDC 2040 to SS 2040	% Change WDC 2040 to SS 2040
287	64	101	56	-45	-44%	1	1	1	-	-	1	1	1	-	-	62	99	54	-45	-45%
312	88	393	209	-184	-47%	-	-	-	-	-	-	-	-	-	-	88	393	209	-184	-47%
313	407	624	514	-110	-18%	355	382	386	4	1%	-	-	-	-	-	52	242	128	-114	-47%
316	117	249	268	19	8%	117	249	268	19	8%	-	-	-	-	-	-	-	-	-	-
317	315	696	468	-228	-33%	-	-	-	-	-	1	2	3	1	28%	314	694	465	-229	-33%
340	311	439	300	-139	-32%	-	-	-	-	-	59	69	72	3	4%	252	370	228	-142	-38%
341	4	18	9	-9	-53%	-	-	-	-	-	1	6	7	1	23%	3	12	1	-11	-90%
342	77	221	139	-82	-37%	-	-	-	-	-	6	10	12	2	22%	71	211	127	-84	-40%
343	26	39	32	-7	-18%	-	1	1	0	15%	-	-	-	-	-	26	38	31	-7	-19%
344	80	129	105	-24	-18%	-	-	-	-	-	15	20	23	3	14%	65	109	83	-26	-24%
345	347	364	359	-5	-1%	349	355	356	1	0%	-	-	-	-	-	-	9	4	-5	-60%
348	142	189	176	-13	-7%	-	-	-	-	-	2	3	4	1	37%	140	186	172	-14	-7%
2251	111	171	101	-70	-41%	1	2	2	0	4%	1	2	2	0	14%	109	167	97	-70	-42%
2252	72	101	75	-26	-26%	-	-	-	-	-	10	16	18	2	10%	62	85	57	-28	-33%
2253	33	45	28	-17	-37%	-	-	-	-	-	10	8	8	-	-	23	37	20	-17	-46%
2254	6	23	13	-10	-44%	-	-	-	-	-	1	8	10	2	24%	5	15	3	-12	-80%
2255	3	11	4	-7	-61%	-	-	-	-	-	1	3	4	1	18%	2	8	1	-7	-90%
2333	61	28	159	131	468%	41	12	74	62	513%	-	-	-	-	-	20	16	85	69	434%
2357	677	677	634	-43	-6%	346	346	200	-146	-42%	28	31	4	-27	-87%	303	300	429	129	43%
2358	142	142	412	270	190%	-	-	158	158	-	-	-	-	-	-	142	142	253	111	78%
2359	97	97	187	90	92%	23	28	37	9	34%	-	-	-	-	-	74	69	149	80	116%
2360	25	39	70	31	80%	-	-	16	16	-	-	-	-	-	-	25	39	55	16	40%
2361	-	27	9	-18	-67%	-	-	4	4	-	-	-	-	-	-	-	27	5	-22	-81%
2362	201	309	300	-9	-3%	2	2	35	33	1643%	-	-	-	-	-	199	307	265	-42	-14%
<b>Total</b>	<b>3,406</b>	<b>5,132</b>	<b>4,628</b>	<b>-504</b>	<b>-10%</b>	<b>1,235</b>	<b>1,378</b>	<b>1,539</b>	<b>161</b>	<b>12%</b>	<b>136</b>	<b>179</b>	<b>167</b>	<b>-12</b>	<b>-7%</b>	<b>2,037</b>	<b>3,575</b>	<b>2,922</b>	<b>-653</b>	<b>-18%</b>



# Planning Commission Meeting

January 20, 2014

## *EXECUTIVE SUMMARY*

### **Item 3: Syracuse City Master Transportation Plan**

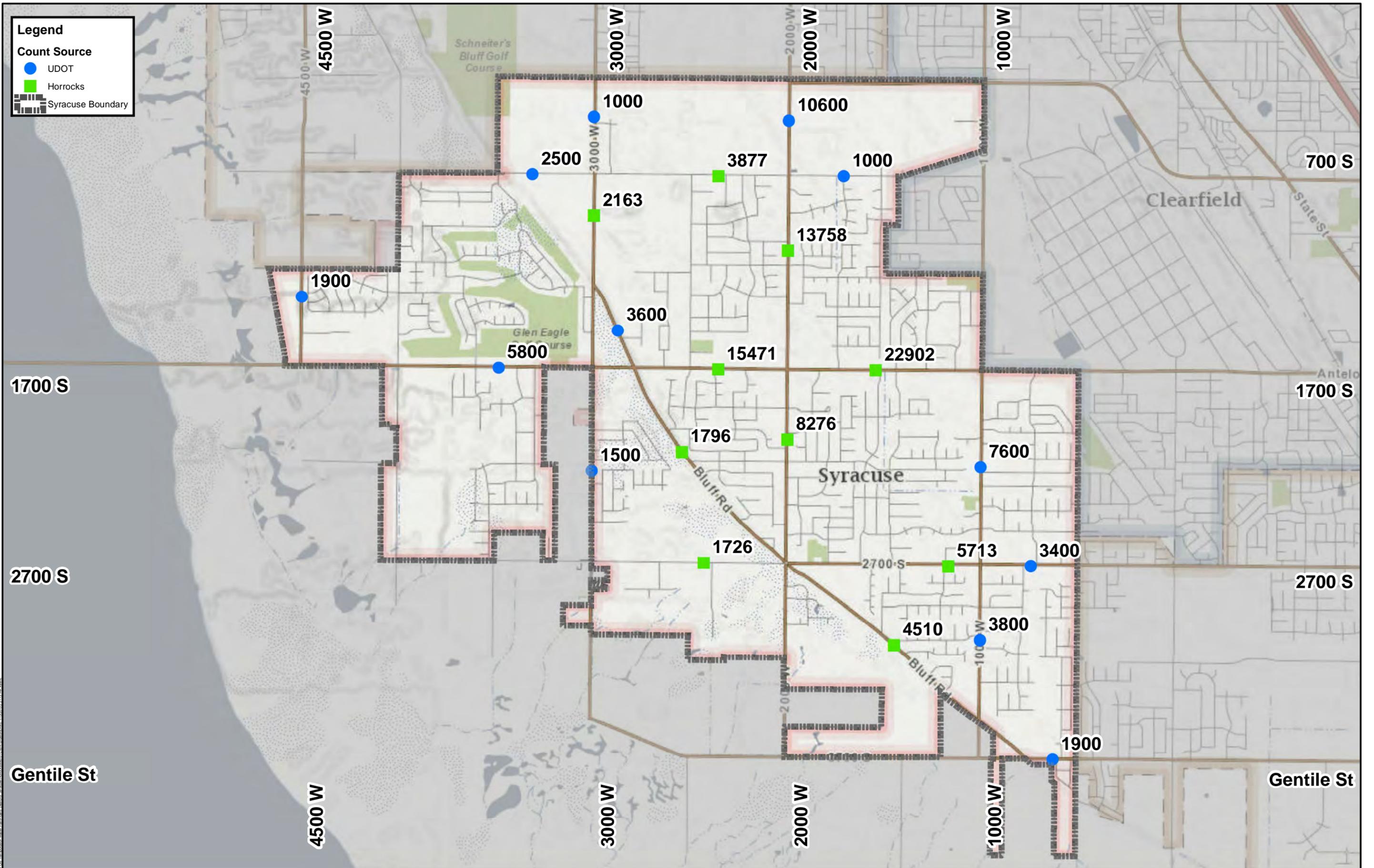
Please refer to the following documents:

- Maps

**Legend**

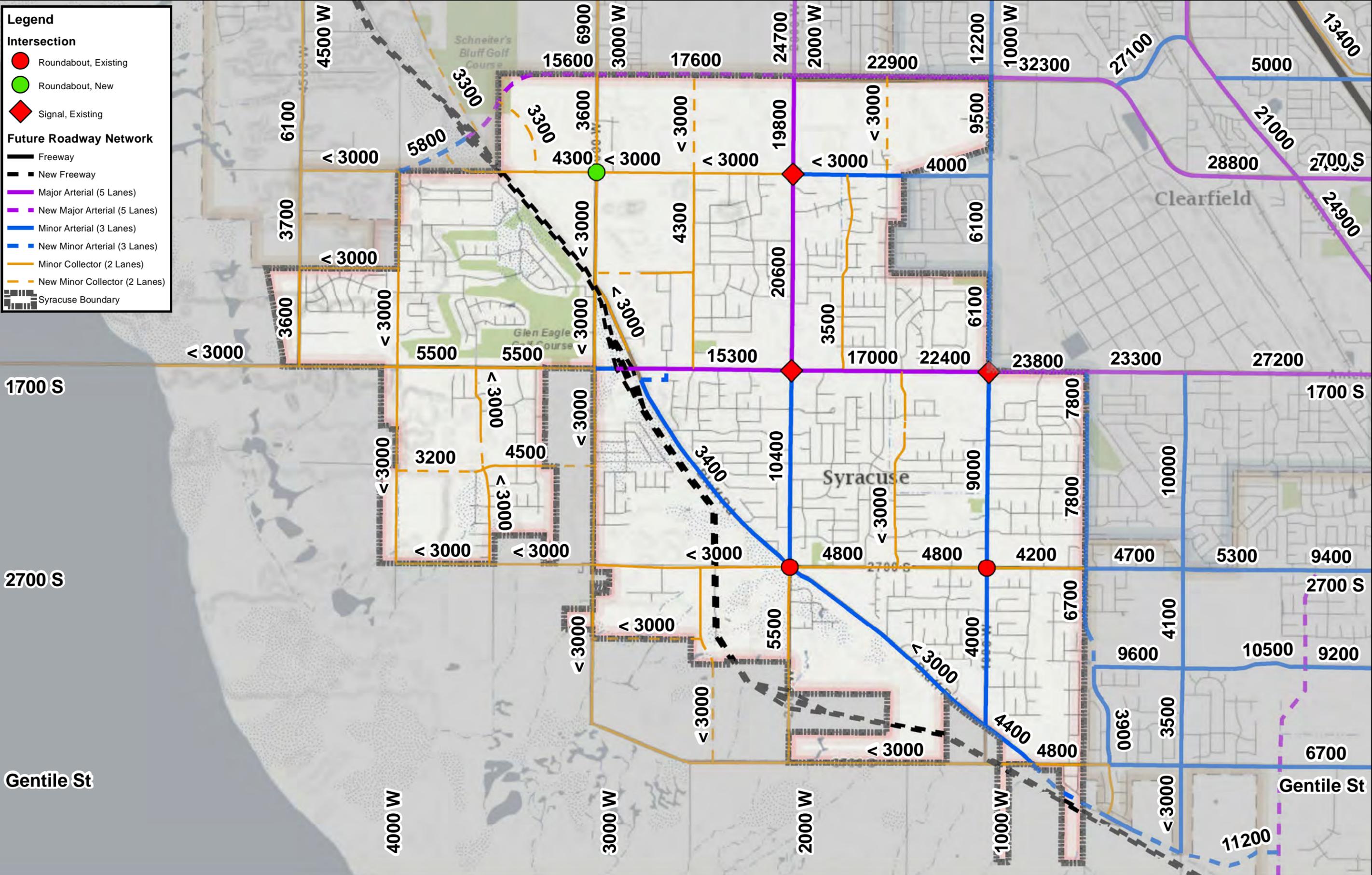
**Count Source**

- UDOT
- Horrocks
- Syracuse Boundary









**Legend**

**Intersection**

- Roundabout, Existing
- Roundabout, New
- ◆ Signal, Existing

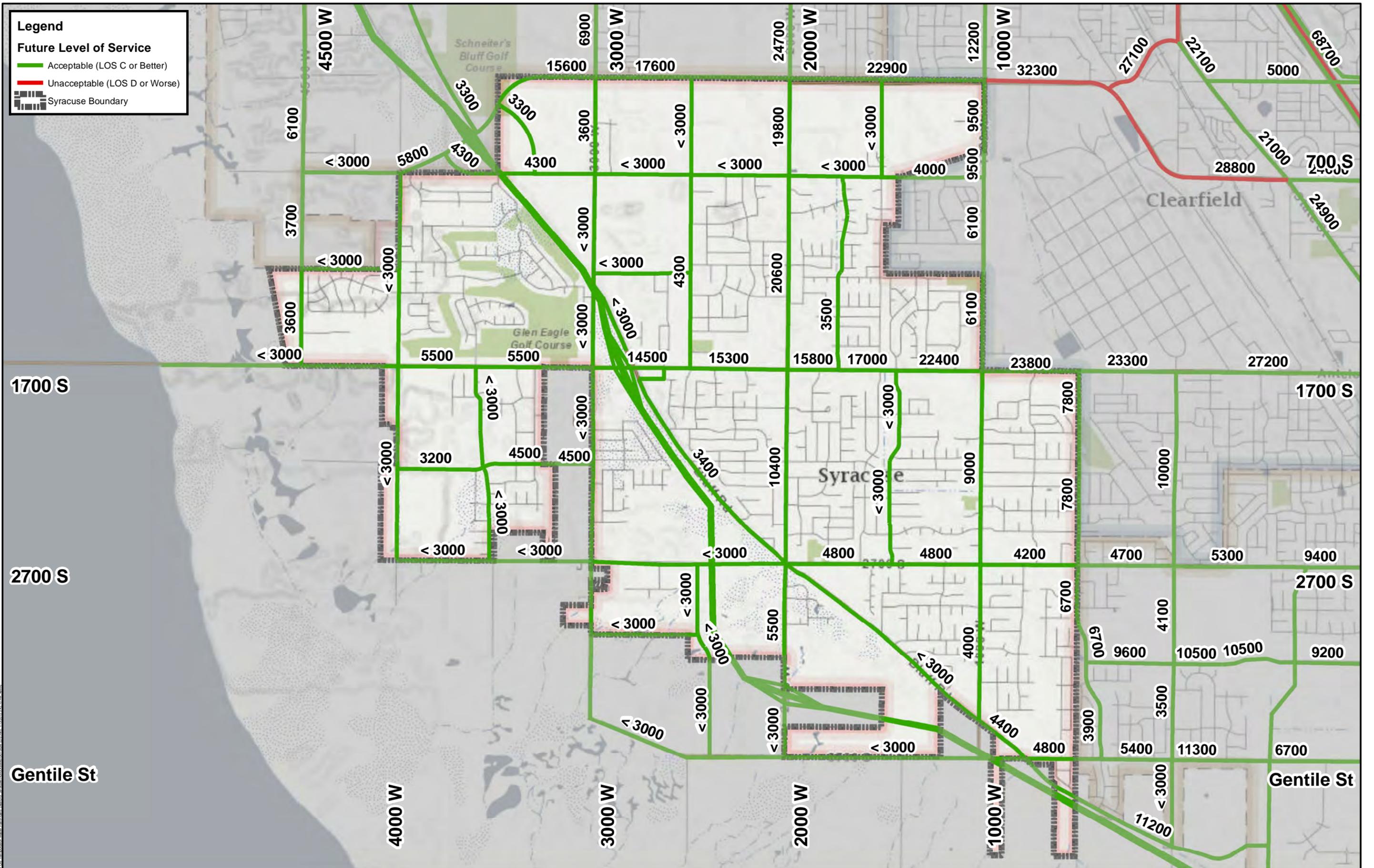
**Future Roadway Network**

- Freeway
- New Freeway
- Major Arterial (5 Lanes)
- New Major Arterial (5 Lanes)
- Minor Arterial (3 Lanes)
- New Minor Arterial (3 Lanes)
- Minor Collector (2 Lanes)
- New Minor Collector (2 Lanes)
- Syracuse Boundary

**Legend**

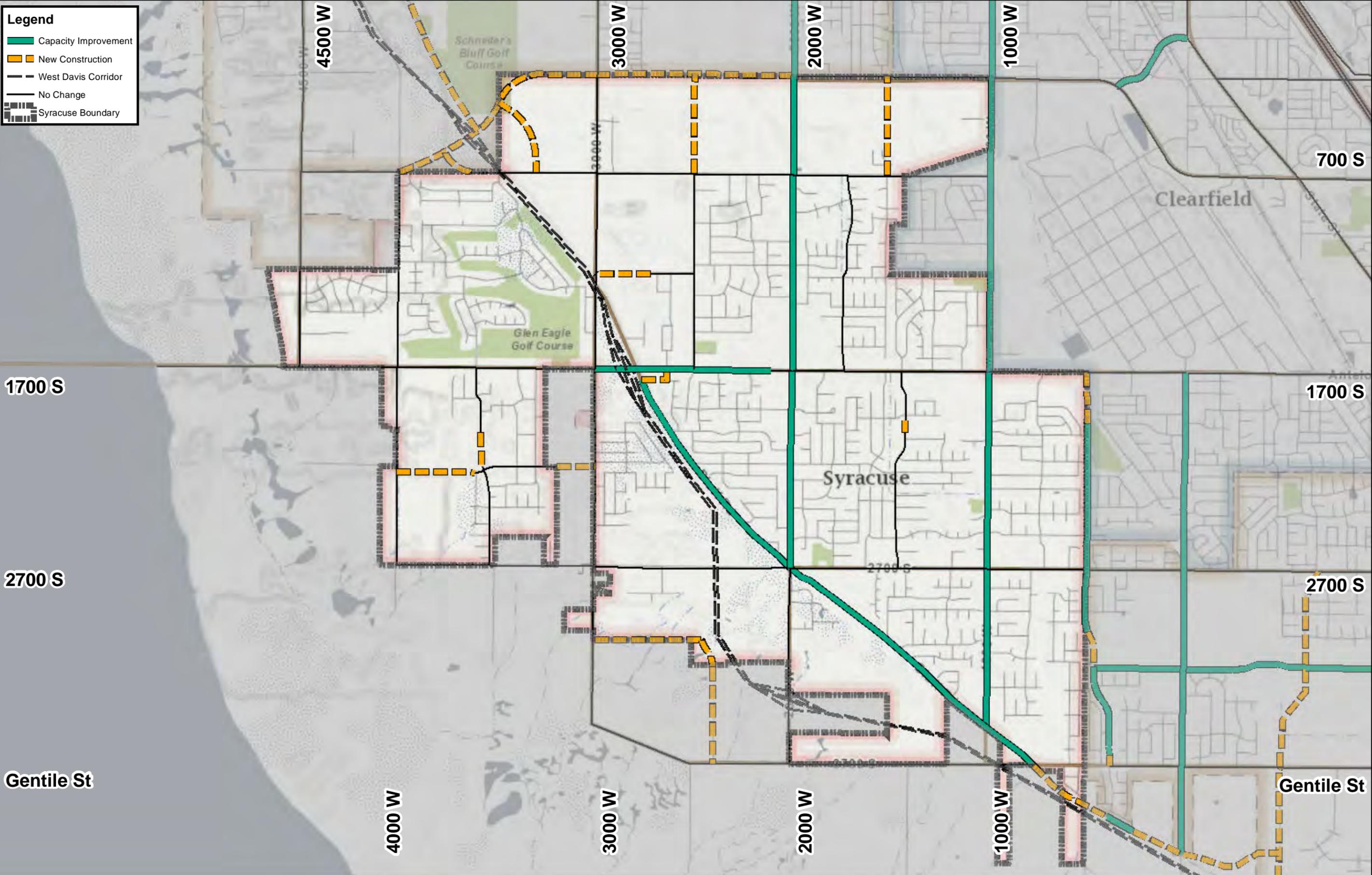
**Future Level of Service**

- Acceptable (LOS C or Better)
- Unacceptable (LOS D or Worse)
- Syracuse Boundary



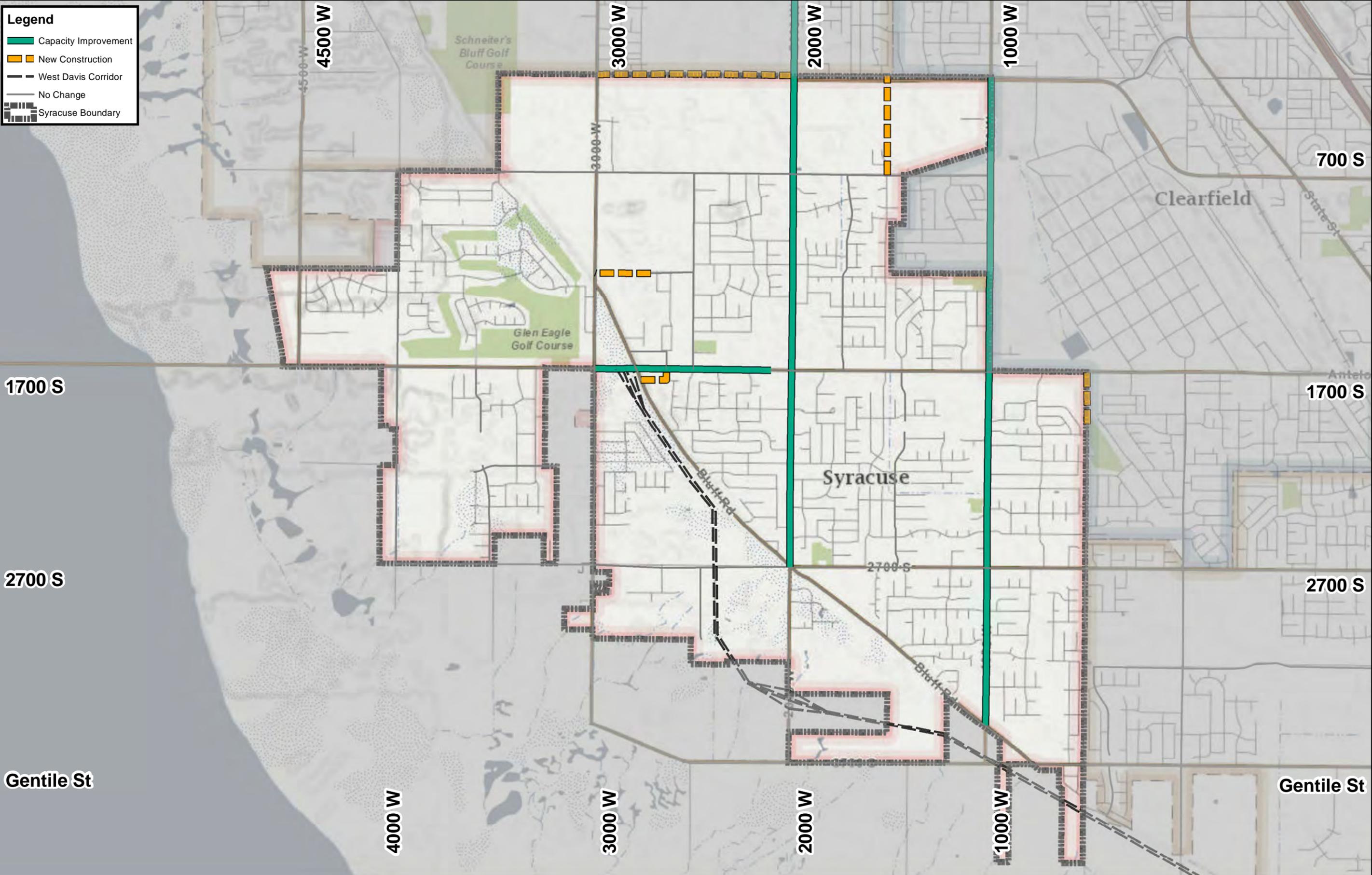
**Legend**

-  Capacity Improvement
-  New Construction
-  West Davis Corridor
-  No Change
-  Syracuse Boundary



**Legend**

- Capacity Improvement
- New Construction
- West Davis Corridor
- No Change
- Syracuse Boundary





SYRACUSE  
EST. CITY 1935

# Planning Commission Meeting

January 20, 2015

## *EXECUTIVE SUMMARY*

### **Item 4a: Code Amendment, Title 10-30-060 regarding fencing regulations**

Please refer to the following documents:

- Applicant Proposal

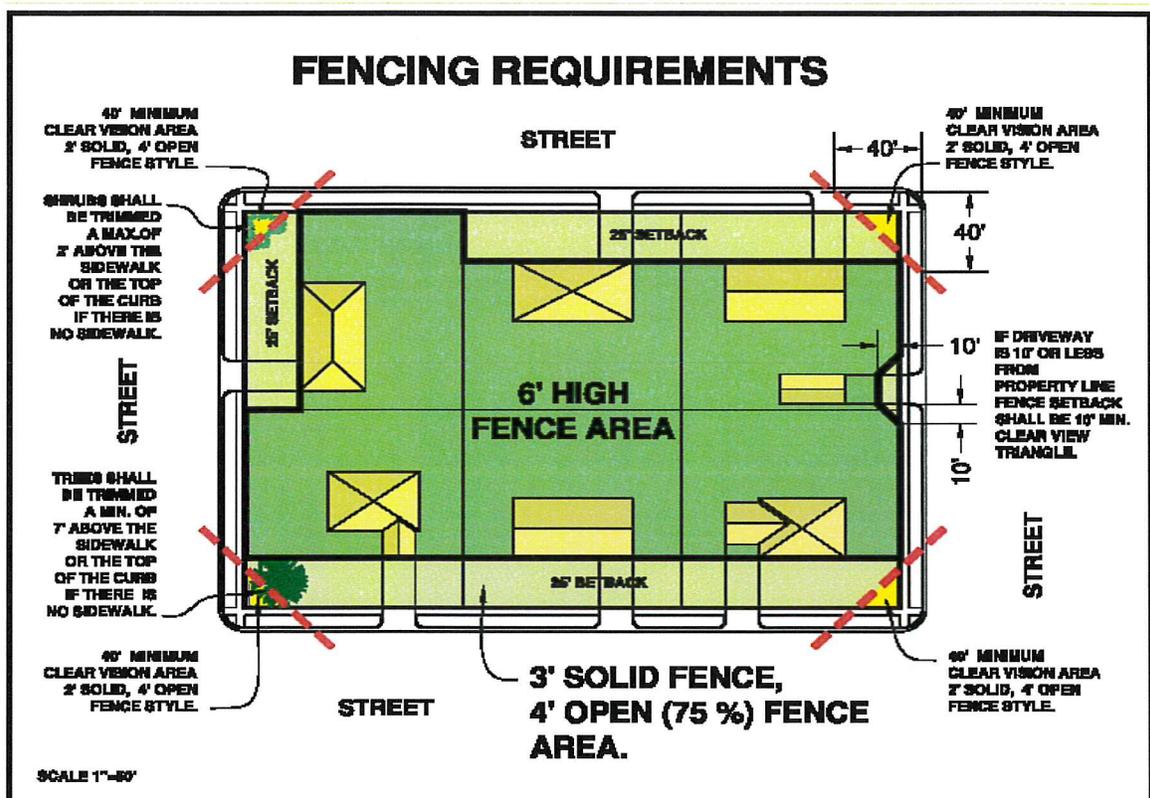
Fencing codes.

Of the fifteen municipalities in Davis County I am unable to find any code where a 6' privacy fence is not allowed along a sidewalk, As long as it does not impede line of sight from corners or driveways. Some examples follow.

Layton City : Code number 19.16.080. Line of sight required on all corner lots. 6' Privacy allowed where not impeding any driveway view.

Fruit Heights: Only code on fencing requires that no fence exceeds 6' in height no line of sight requirements.

South Weber: Fences, walls or similar devices cannot exceed six feet in height. Corner lots must maintain 15' line of sight from centerline of driveway.



We have been using this formula for over fifteen years in the fencing industry and in the majority of situations it is accepted



Brady Parker Estimator Ace Fab & Welding,

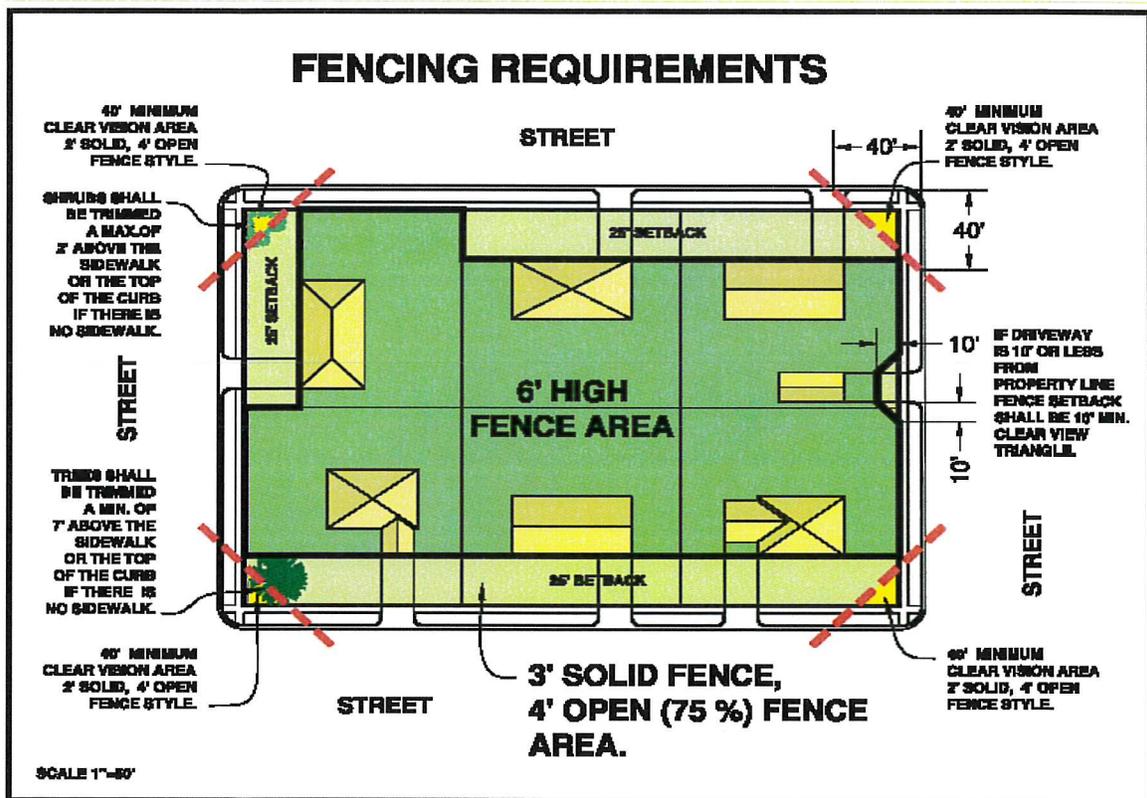
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Brady Parker Estimator Ace Fab & Welding,

### 10.30.060 Miscellaneous requirements and provisions.

(A) Height of [Fences](#), Walls, and Hedges in Residential [Zones](#). Property owners may erect [fences](#), walls, and hedges to the permitted [building height](#) in the [zone](#) in which they are located provided they are not within any required [yard](#) space. [Fences](#), walls, or hedges in any required side and [rear yard](#) shall not exceed six feet in height. [Fences](#) within the required [front yard](#) setback or side street [setback](#) shall not exceed <sup>six feet</sup> ~~three feet~~ in height and shall comply with the requirements of subsection (B) of this section. Where a retaining wall is reasonable and necessary, located on the line separating lots, such retaining wall may include a [fence](#), wall, or hedge on top to the same height that would otherwise be permitted if no retaining wall existed.

(B) Visibility at Intersections. Notwithstanding any provisions of this title in any district, [fences](#), walls, [signs](#), hedges, or other plantings may not encroach within the required clear-visibility triangle located at the intersection of two roads or the intersection of a road and a nonresidential driveway. No one shall erect, place, or plant anything or allow anything to grow in such a manner as to impede vision between a height of two and one-half feet and eight feet above centerline grades of the adjoining street centerline surface elevation. Property owners shall maintain all tree branches so that none of them are lower than eight feet in clear site triangle [area](#). The configuration of a clear-visibility triangle is shown in Exhibit 10.30.060, and the length of each leg, measured along the curb line or edge of roadway or edge of nonresidential driveway, shall be 40 feet, or based upon applicable speed limits greater than 40 mph as per AASHTO's "Policy on Geometric Design of Highways and Streets."



SYRACUSE  
EST. CITY 1935

# Work Session

January 20, 2014

## *EXECUTIVE SUMMARY*

**Item 4b: Title X Code amendment pertaining to PRD, Planned Residential Development.**

Please refer to the following documents:

- Proposed Amendment

## A-1 Zone

### 10.55.040 Minimum lot standards.

Developers shall improve all lots and place all structures and uses on lots in accordance with the following lot standards. Lot areas for properties fronting existing streets shall include all property as described on the most recent plat of record.

(A) ~~Density.~~ Minimum lot size 21,780 square feet, ~~but in no case shall the density exceed 0.5 lots per net acre, unless the Land Use Authority grants additional density, per a major conditional use permit, up to a maximum of 2.5 lots per net acre.~~

## R-1 Zone

### 10.60.040 Minimum lot standards.

All lots shall be developed and all structures and uses shall be placed on lots in accordance with the following standards. Lot area for properties fronting existing streets shall include all property as described on the most recent plat of record.

(A) ~~Density.~~ Minimum lot size ~~10,000~~12,000 square feet, ~~but in no case shall the density exceed 2.9 lots per net acre, unless the Land Use Authority grants additional density, per a major conditional use permit, up to a maximum of 4.75 lots per net acre.~~

## R-2 Zone

### 10.65.040 Minimum lot standards.

All lots shall be developed and all structures and uses shall be placed on lots in accordance with the following standards:

(A) ~~Density: minimum~~Minimum lot size 10,000 square feet, ~~but in no case shall the density exceed 3.79 lots per net acre.~~

## R-3 Zone

### 10.70.040 Minimum lot standards.

All lots shall be developed and all structures and uses shall be placed on lots in accordance with the following standards:

(A) ~~Density: m~~Minimum lot size 8,000 square feet, ~~but in no case shall the density exceed 5.44 lots per net acre.~~

## **Chapter 10.75**

### **PRD – PLANNED RESIDENTIAL DEVELOPMENT**

Sections:

- 10.75.010 Purpose.
- 10.75.020 Permitted uses.
- 10.75.030 Conditional uses.
- 10.75.040 Minimum lot standards.
- 10.75.050 Development plan and agreement requirements.
- 10.75.060 Design standards.
- 10.75.070 Street design.
- 10.75.080 Off-street parking and loading.
- 10.75.090 Signs.

#### **10.75.010 Purpose.**

The purpose of this zone is to allow diversification in the relationship of residential uses to their sites and permit directed flexibility of site design. Further, its intent is to encourage a more efficient use of the land and the reservation of a greater proportion of common space for recreational and visual use than other residential zones may provide and to encourage a variety of dwelling units that allow imaginative concepts of neighborhood and housing options and provide variety in the physical development pattern of the City. This will allow the developer to more closely tailor a development project to a specific user group, such as retired persons.

The intent of this zone is to encourage good neighborhood design while ensuring compliance with the intent of the subdivision and zoning ordinances. All dwelling units are to be held in private individual ownership. However, the development shall contain common or open space and amenities for the enjoyment of the planned community that are developed and maintained through an active homeowners' association or similar organization with appointed management. [Ord. 12-01 § 1; Ord. 11-04 § 6; Ord. 11-02 § 1 (Exh. A); Ord. 08-07 § 1 (Exh. A); Ord. 06-27; Ord. 06-17; Code 1971 § 10-15-010.]

#### **10.75.020 Permitted uses.**

The following are permitted uses by right provided the parcel and building meet all other provisions of this title and any other applicable ordinances of Syracuse City:

- (A) Accessory uses and buildings (under 200 square feet).
- (B) Churches, synagogues, and temples.
- (C) Dwelling units, single-family (no more than four units attached).

(D) Educational services.

(E) Household pets.

(F) Private parks.

(G) Public and quasi-public buildings.

(H) Residential facilities for persons with disabilities and assisted living centers. [Ord. 12-01 § 1; Ord. 11-04 § 6; Ord. 11-02 § 1 (Exh. A); Ord. 08-07 § 1 (Exh. A); Ord. 06-27; Ord. 06-17; amended 1991; Code 1971 § 10-15-020.]

#### **10.75.030 Conditional uses.**

The following may be permitted conditional uses after approval as specified in SCC 10.20.080.

(A) Day care centers (major).

(B) Home occupations (minor or major).

(C) Temporary commercial uses (see SCC 10.35.050) (minor).

(D) Temporary use of buildings (see SCC 10.30.100(A)(9)) (minor). [Ord. 12-01 § 1; Ord. 11-04 § 6; Ord. 11-02 § 1 (Exh. A); Ord. 08-07 § 1 (Exh. A); Ord. 06-27; Ord. 06-17; amended 1991; Code 1971 § 10-15-030.]

#### **10.75.040 Minimum lot standards.**

All lots shall be developed and all structures and uses shall be placed on lots in accordance with the following standards:

(A) Density: The City Council, upon recommendation from the Planning Commission on Preliminary Plat, shall determine the allowed dwelling unit density up to eight (8) dwelling units per net acre, building setbacks, and minimum lot size through a development plan based on the specific merits of the proposed development as well as on factors such as recreation facilities, greater open space, landscaping features, fencing type and design, signage, clubhouse provisions, homeowners' covenants, professional maintenance, trails/pathways, and quality of exterior building materials. However, Condominium developments shall comply with the Utah Condominium Act, but in no case shall the overall density of the development exceed eight dwelling units per net acre, without recommendation for approval by the Planning Commission and the consent and approval of the City Council. Calculation of net acreage shall be that portion of the parcel remaining after street right-of-ways have been deducted from the overall parcel(s).

~~The overall density of the development may exceed eight dwelling units per net acre and increase up to a maximum of 12 dwelling units per net acre only after receiving recommendation for approval by the Planning Commission and consent and approval by the City Council. The Planning Commission recommendation and City Council consent and approval, for a developer to exceed eight dwelling units per net acre, shall be subject to the ability of the development plan to meet the following criteria:~~

~~(1) The development area shall be a transitional residential buffer to commercial, industrial, and/or retail zones, as established in the general plan;~~

~~(2) The development shall provide a standard road right-of-way of 60 feet which shall include curb, gutter, and sidewalk improvements;~~

~~(3) The development shall provide a minimum of 35 percent parks and/or functional open space within the development based on the net acreage of the proposed development;~~

~~(4) The aesthetic and landscaping proposals shall provide a superior residential development and environment;~~

~~(5) The development shall provide adequate off-street parking area(s), subject to requirements of this chapter and off-street parking requirements as found in Chapter 10.40 SCC; and~~

~~(6) The development design shall include a direct connection to a major arterial, minor arterial, or major collector roadway.~~

~~(1) Single family lots shall only be allowed with a minimum lot size of 5,500 sq. ft. and 55 feet of frontage, with a maximum net density of 4 dwelling units per net acre.~~

~~(2) Single family detached patio homes surrounded by common area/open space shall have a maximum density of 6 dwelling units per net acre.~~

(B) Lot width: determined by development plan.

(C) Front yard: 20 feet.

(D) Side yards: a minimum of 16 feet between ~~attached~~ units.

(E) Rear yard: a minimum of 15 feet.

(F) Building height: as allowed by current adopted building code, with a maximum height of 30 feet to the top of the roof structure.

(G) Open space/Common Area: a minimum 50 percent of the total land net area, excluding ~~roadways, buildings, right of ways,~~ and above-ground City infrastructure. [Ord. 12-01 § 1; Ord. 11-04 § 6; Ord. 11-02 § 1 (Exh. A); Ord. 08-07 § 1 (Exh. A); Ord. 06-27; Ord. 06-17; amended 1998; Code 1971 § 10-15-040.]

#### **10.75.050 Development plan and agreement requirements.**

(A) Subdivision ordinance requirements shall generally apply to planned residential communities. The developer shall submit a residential development plan of all project phases for City consideration and approval and shall integrate the proposed development plan into a development agreement between the developer and City. The development agreement shall undergo an administrative review process to ensure compliance with adopted City ordinances and standards with approval by the City Council. The subdivider shall develop the property in accordance with the development agreement and current City ordinances in effect on the approval date of the agreement, together with the requirements set forth in the agreement, except when federal, state, county, and/or City laws and regulations, promulgated to protect the public's health, safety, and welfare, require future modifications under circumstances constituting a rational public interest. The ~~Land Use Authority~~City Council shall use the submitted development plan and agreement with the design amenities and unique development features and merits of the development to determine overall development dwelling unit density up to a maximum of ~~12~~8 dwelling units per net acre.

(B) A planned residential development must have a minimum of five acres ~~with a minimum of 20 percent of the acreage in common space area excluding required roadways, curbs, and other City infrastructure.~~

(C) The developer shall landscape and improve all open or common space around or adjacent to building lots and maintain the same through a lawfully organized homeowners' association, residential management company, or similar organization. Open/Common Space shall be improved with amenities that benefit the residents of the development. No more than 20% of the required open/common space shall be improved with enclosed buildings or structures, an example of such is a community club house.

(D) The development plan submitted for review shall show the location and building elevations with exterior building materials, size, and general footprint of all dwelling units and other main buildings and amenities.

(E) The development plan submitted for review shall include landscaping, fencing, and other improvement plans for common or open spaces, with the landscaping designed in accordance with an approved theme to provide unity and aesthetics to the project. The plan shall include all special features, such as ponds, fountains, signs, walking paths, inviting entryways, etc., together with a landscape planting plan. Open space and recreational areas should be the focal point for the overall design of the development, with various community facilities grouped in places well related to these open spaces and easily accessible to pedestrians.

(F) The proposed development shall show it will not be detrimental to the health, safety, or general welfare of persons residing adjacent to the proposed development.

(G) A planned residential community shall be of sufficient size, composition, and arrangement to enable its feasible development as a complete unit, managed by a legally established owners' association and governed by enforceable, duly recorded CC&Rs. [Ord. 12-01 § 1; Ord. 11-04 § 6; Ord. 11-02 § 1 (Exh. A); Ord. 08-07 § 1 (Exh. A); Ord. 06-27; Ord. 06-17; Code 1971 § 10-15-050.]

**10.75.060 Design standards.**

The Land Use Authority shall approve the required common building theme. The design shall show detail in the unification of exterior architectural style, building materials, and color and size of each unit; however, the intent is not to have the design so dominant that all units are identical. Residential dwellings shall comply with SCC 10.30.020. [Ord. 12-01 § 1; Ord. 11-04 § 6; Ord. 11-02 § 1 (Exh. A); Ord. 08-07 § 1 (Exh. A); Ord. 06-27; Ord. 06-17; Code 1971 § 10-15-060.]

**10.75.070 Street design.**

The Land Use Authority may approve an alternative street design so long as it maintains the City's minimum rights-of-way. The developer shall dedicate all street rights-of-way to the City. [Ord. 12-01 § 1; Ord. 11-04 § 6; Ord. 11-02 § 1 (Exh. A); Ord. 08-07 § 1 (Exh. A); Ord. 06-27; Ord. 06-17; Code 1971 § 10-15-070.]

**10.75.080 Off-street parking and loading.**

Off-street parking and loading shall be as specified in Chapter 10.40 SCC; provided, however, that the City may limit or eliminate street parking or other use of City rights-of-way through the employment of limited or alternative street designs. [Ord. 12-01 § 1; Ord. 11-04 § 6; Ord. 11-02 § 1 (Exh. A); Ord. 08-07 § 1 (Exh. A); Ord. 06-27; Ord. 06-17; amended 1991; Code 1971 § 10-15-080.]

**10.75.090 Signs.**

The signs permitted in this zone shall be those allowed in residential zones by Chapter 10.45 SCC. [Ord. 12-01 § 1; Ord. 11-04 § 6; Ord. 11-02 § 1 (Exh. A); Ord. 08-07 § 1 (Exh. A); Ord. 06-27; Ord. 06-17; amended 1991; Code 1971 § 10-15-090.]



# Work Session

January 20, 2014

## *EXECUTIVE SUMMARY*

### **Item 4c: Title X Code amendment pertaining to Architectural Review Committee and Design.**

Please refer to the following documents:

- Proposed Amendment
- Design Standards Example Guide

## Chapter 28

# ARCHITECTURAL REVIEW COMMITTEE AND DESIGN STANDARDS (Ord. 13-11)

### Sections:

#### 10.28.010 Purpose

#### 10.28.020 Architectural Review Committee (ARC)

#### 10.28.100 Commercial, Professional Office, **Multifamily Residential** and Public Facility Developments

#### 10.28.110 Building Design

#### 10.28.120 Site Design

#### 10.28.200 Industrial Developments

#### 10.28.210 Industrial Site Planning

#### 10.28.220 Industrial Architecture

#### 10.28.230 Industrial Landscape Design

#### 10.28.010 Purpose.

- (A) The purpose of the design standards outlined in this Chapter are to facilitate exceptional design in the built environment and preserve the quality of life within Syracuse City. This Chapter is based on the following principles:
1. Higher quality designs will bring value to the commercial community of Syracuse and attract businesses which positively contribute to the overall quality of life and well-being of Syracuse residents.
  2. Syracuse welcomes a variety of businesses and recognizes that brand recognition has become an important aspect of developing a successful and competitive business. The design standards enumerated in this Chapter are intended to help and encourage development of imaginative and unique design solutions which meet business needs, while at the same time preserving Syracuse's unique history and community identity.
  3. This Chapter supplements the objectives of other chapters in this Title which also impose certain design standards or requirements that are zone specific.
- (B) This Chapter references design standards and **a design guidelines standards example guide**. They are intended to improve the quality and compatibility of development, particularly with regard to building design, and site design.
1. Design Standards. Design standards are required in addition to other standards set forth in this Title. Design standards are intended to ensure quality development and permanence in design. The implementation of these standards ensures that the goals and values of the community are reflected in each commercial **and multi-family residential** development and contribute positively to the overall built environment.
  2. Design ~~Guidelines~~ **Standards Example Guide**. ~~Design guidelines~~ **The Guide** offers examples to clarify and explain **the** design standards. ~~They~~ **The Guide** ~~are made~~ **is** available for public use and distribution within a separate manual that can be found on

record in the Community Development Department. The purpose of the Syracuse City Commercial Design Standards and Guidelines Manual Guide is to better clarify the relationship between design standards and guidelines. The design guidelines serves to graphically represent the design standards and are is used as a reference tool in order to better illustrate the application of the design standards.

#### **10.28.020 Architecture Review Committee (ARC).**

The ARC is established to review all applicable development plans for compliance with the design standards in this chapter. This Committee functions as a subcommittee of the Planning Commission and consists of seven (7) members appointed by the Mayor with the advice and consent of the City Council in accordance with the Syracuse Municipal Code. Members consist of community residents, Planning Commissioners (maximum 2), and City staff. The Planning Commission Chair may recommend candidates for the Mayor's consideration.

The ARC is responsible to review the plan, elevations, architectural details, and development design pattern book and make recommendations to the Planning Commission consistent with this Chapter. The Planning Commission will, in turn, submit their recommendations to the City Council.

#### **10.28.100 Commercial, Professional Office, Multifamily Residential, and Public Facility Developments.**

(A) Applicability. The design standards set forth in this Part apply to:

1. All new commercial, professional office, multifamily residential or public facilities buildings and uses except where the requirements of this Chapter are superseded by another provision of this Title.
2. Existing commercial, professional office, multifamily residential, or public buildings and uses when a change occurs that involves a design standard set forth in this Chapter and either;
  - (a) requires a building or other permit issued by the City;
  - (b) alters the occupancy designation of a building under the current building codes adopted by the City;
  - (c) increases required parking; or
  - (d) increases the amount of outside storage.

(B) Imposition of Conditions. When reviewing a development plan, the Architecture Review Committee ("ARC") may recommend that the Planning Commission impose conditions consistent with design standards in this Chapter.

(C) Other Code Requirements. The requirements of this Chapter apply in addition to other applicable requirements of this Title.

#### **10.28.110 Building Design.**

(A) Context. All building designs are required to be drafted with sensitivity to the design of nearby buildings and developments. This sensitivity includes considering whether:

1. buildings contribute to the overall character of Syracuse and the neighborhood;
2. buildings reflect the character of surrounding development through use of some similar features;
3. buildings demonstrate imaginative design; and

4. buildings vary colors, materials, or architectural elements where proposed building designs closely copy or mirror surrounding developments.
- (B) Entrances. All building entrances are required to be clearly articulated to indicate a transition from the exterior to the interior of the building. Every main entrance is required to have a special emphasis when compared to the other portions of the building. This is accomplished through the use of at least three of the following near entrances:
1. a prominent architectural feature that is unique to the overall building design;
  2. complimentary yet differing building materials or colors;
  3. increased use of windows or glass;
  4. pedestrian amenities that may include patios, porches, special paving treatments, seating areas, or awnings; or
  5. increased landscaping.
- (C) Façade Articulation. Buildings designed with completely flat façades and monotone color schemes are not permitted. All buildings are required to have articulation of all façades.
1. Horizontal or vertical façade variations must occur at least every 30 feet or along a minimum of 60% of the horizontal length of buildings with facades 100 feet or greater. This is accomplished by using methods such as:
    - (a) variation in the surface plane that may include pop-outs, bays, and recesses;
    - (b) variation in the surface pattern such as arches, banding, and paneling; or
    - (c) distinguished treatment of windows, doors, and eaves that may include molding or framing.
  2. Buildings with façades 100 feet or greater in total length must have at least one significant façade variation from the primary wall plane whose depth is at least 5% of the total façade length and whose width is at least 20% of the total façade length. Uninterrupted façades 100 feet or greater in length are prohibited.
- (D) Height and Roofline. All building roof heights will be compatible with the building's location, and varied roofline elevations or parapets are required in order to add architectural interest and avoid the appearance or sense of monotonous roofline expanses. All buildings are required to:
1. have appropriate roof height for the location based on zoning regulations and the height of buildings within the immediate area;
  2. have roofline and parapet variations where there are long, continuous, and undisturbed rooflines 50 feet in length or greater;
  3. use similar materials and colors on the back of false-fronts, parapets, cornices, or other parts of the building which extend beyond the roofline or main building so that the building appears cohesive from all views; and
  4. have screening of mechanical equipment and systems that will be mounted on the roof. Any portion of these pieces of equipment that is not fully shielded is required to be painted a color which is compatible with the roofing or parapet materials.
- (E) Massing. Proper massing reduces the impact of the massive bulk created by large buildings that may not otherwise relate in scale to surrounding development. Vertical

articulation, horizontal articulation, and multi-planed roof or awnings must be used in designs to mitigate the impact on surrounding development and the overall landscape.

- (F) Materials. Quality long-lasting materials are required for all buildings in order to contribute to the aesthetics of the community over the long term.
1. A minimum of three colors per elevation is required.
  2. Color utilization should be sensitive to existing development within the vicinity and the natural landscape in which the project is situated.
  3. Primary Materials. 65% of all surface materials, not including glass or roofing materials, are required to include a combination of brick, stone, ceramic tile, masonry materials, insulated metal panels, or wood fiber/composite cement-fiber siding. Exposed cinder block is not permitted, except for minimal foundation exposure. Unfinished or painted concrete block is prohibited as a surface material. Architectural block such as split faced, honed, or similar may be permitted. Concrete masonry unit, Exposed concrete, stucco, vinyl, wood siding, or insulated metal panels may be used as accent or secondary materials only.
  - ~~4. Exposed tilt-up concrete or insulated metal panels may be used as a primary material on buildings located in business park zone. Some variation in materials along the base and near the entrances of concrete tilt-ups is required.~~
  - 4-5. All projects are required to submit a sample board containing physical samples of all exterior surface materials, including roofing materials, in all the colors they will be used. Photos alone are not sufficient.
- (G) Development Design Pattern Book. The developer is required to provide a development design pattern book ~~to be reviewed by the ARC and then the Planning Commission in conjunction with a subdivision plan and/or site plan application.~~ for projects including more than one structure. ~~Where there is a development agreement, the design pattern book will become a part of the agreement.~~ Design pattern books are subject to the following:
1. Written descriptions with graphic illustrations explaining how the development complements the physical form of the property and how the theme and standards found in this Chapter are to be integrated into the design of the development.
  2. Written descriptions with graphic illustrations explaining the proposed conceptual architectural design, building elevations, and other such related design schemes; and
  3. Written descriptions with graphic illustrations that clearly describe proposed open spaces, landscaping ideas, pedestrian pathways, furnishings, lighting and related entryway features and/or amenities.
- (H) Pedestrians. All buildings will be designed with an integral focus on encouraging pedestrian activity and social interaction. Additionally, buildings that contain more than one story or that are above 20 feet in height are required to provide a clearly articulated and more detailed base that relates to pedestrians.
- (I) Signs. Signs located on any building façade are required to be compatible with the building's overall design. As an integral design element, signs are required to be compatible with the style of the buildings in terms of location, scale, color, and lettering.
1. The locations for signs on a building's façade will be planned for as part of the building's overall design.
  2. Signs located on façades should integrate similar or complimentary materials as the building.

- (J) Windows. Windows are key to the overall design of a building and the relationship between the exterior and interior. The majority of windows are required to relate to the scale of a person.
1. Windows should be at eye-level .
  2. Where buildings are adjacent to pedestrian walkways, transparent windows must relate the scale of the building and the building's interior to pedestrians. Where transparent windows may not be desirable, tinted windows, false windows, or glass block are allowed.
  3. Window awnings are an effective way to add detail and variation to a building, emphasize pedestrian scale windows, and create an exciting and inviting environment. When placing awnings:
    - (a) use quality materials which are durable and are able to withstand extremes in the weather;
    - (b) colors should be complimentary to the color of the building on which the awning is located; and
    - (c) styles should be compatible with the architectural features and overall architecture of the building on which the awning is located.

#### **10.28.120 Site Design.**

- (A) Building Placement. Building placement is integral to the site design and the overall effect any development has on surrounding properties. Building placement is required to comply with the following:
1. All buildings must be oriented with the main or similar façade facing a principal street to which it has frontage.
  2. Buildings located on corner lots are required to orient main façades to each street and give equal treatment to each.
  3. Buildings located on corner lots should include a prominent architectural feature of greater height than the rest of the roof, or emphasis at the corner where the two public streets meet.
- (B) Context. New developments are required to match or compliment surrounding developments **and landscapes** in order to create a site which relates to its surroundings and adds positively to the overall ~~built~~ environment in the site area.
1. **All developments shall consider natural features, such as view corridors, riparian corridors, creeks, topography, mature trees, and vegetation, as integral features to be preserved, enhanced, and incorporated into development proposals.**
  2. **Structures which are historic or are otherwise distinctive should be preserved and incorporated into development proposals.**
- (C) Miscellaneous. Screening: The small details that sometimes become afterthoughts of site planning and design are important factors for making aesthetically attractive sites. The following are required during initial site planning, along with any other requirements within this code:
1. Landscaped areas and walls will be designed to decrease noise levels and separate loading and service bays from customer parking. Service and loading bays

(automotive, service, tire, etc.) should be oriented away from neighboring residential areas. Further they cannot have direct access to major public streets, access shall be provided via internal site plan circulation. The general public should be restricted from accessing such areas.

2. Approved outdoor storage areas are required to be fully screened from view using the same materials as the building or approved masonry fencing. Enclosures are also required to be surrounded by landscaping to further soften their visual impact. Public access to these areas should be restricted.
3. Outdoor refuse and garbage collection containers are required to be fully screened from view using the same materials as the building or approved masonry fencing. Enclosures are required to be surrounded by landscaping to further soften their visual impact. Public access to these areas should be restricted.
4. Shopping cart corrals are highly visible and needed to keep development sites safe and orderly. Corrals are required to be in fixed position.
5. Generators are required for businesses that the ARC determines will provide essential services to the public during inclement weather or natural disasters. Generators and other large auxiliary equipment are required to be placed at locations where they will be least intrusive in terms of noise, appearance, and odors, particularly for occupants of neighboring properties. Site plans will include public utilities equipment placement. Generators and other large auxiliary equipment:
  - (a) should be kept low to the ground;
  - (b) if they include exhaust systems may not have exhaust systems protrude above the equipment; and
  - (c) are required to be surrounded by walls, landscaping, and other screening.
6. **Razor and barbed wire fences are prohibited if visible from public right-of-way or a residential area, except for agricultural purposes. Chain link fencing shall be coated in a non-obtrusive color, such as black or dark green, in order to diminish its visual impact.**
6. ~~Site plans will be designed to conform to all requirements of the American's with Disabilities Act (ADA). The ARC shall review the site plan for adequate handicap parking and access to buildings.~~

(D) Landscaping. Landscaping is required as a tool to enhance and beautify the site, and the building's architecture and design. To aid in the design process, designers are encouraged to refer to the ~~Syracuse City Commercial Design Standards and Guidelines Manuals~~ **Design Standards Example Guide**.

1. The portion of a development site devoted to landscaping exclusive of parkstrips and walkways will be calculated by the zone requirements for the parcel. Additional landscaping should be provided to achieve compatibility between differing, adjacent land uses such as residential and commercial, as required in the buffering table.
2. ~~Landscaping is required to be integrated into the design, placement, and enhancement of pedestrian plazas, sitting areas, walkways, flag poles, and signs and will comply with zoning requirements.~~ **Outdoor amenities such as patios, plazas, public art, water features, and outdoor seating areas are encouraged.**
3. ~~Vegetative ground cover must be utilized for at least 80% of a development site's frontage along any public right-of-way and in required landscape areas between a building and the parkstrip not including sidewalks, trails, and entry points.~~ **One (1) street tree shall be planted in the park strip or front set-back every 30 feet of property**

frontage abutting any public street. Tree spacing should be equidistant and coordinate with adjacent street trees, with the purpose of creating tree lined streets.

4. Where possible and reasonable, existing landscape features and mature trees should be preserved and incorporated into landscape plans.
5. A minimum 2' wide landscape strip of woody landscape plants is required around the base of all structures, exclusive of sidewalks or driveways that cross at a perpendicular angle.
6. Drought resistant shrubs and trees will be used in conjunction with efficient low water use irrigation systems. A list of low water use plants is available from the Community Development Department.
7. Invasive or noxious plants & weeds shall be removed during site preparation.

(E) Lighting. Carefully planned lighting schemes can create safe environments for pedestrians and motorists. Lighting is an integral design element which adds to the overall site plan and building design. Lighting must be designed in compliance with Chapter 8 (Signs and Lighting Regulations).

1. Fixtures shall match existing down facing "Bell" style or similar fixtures found in the town center. Lighting schemes will include coordinating parking, wall, and pedestrian scaled fixtures which compliment building architecture and site features.
2. Parking and Circulation. Lifestyle, separated land uses, and lack of sufficient public transit have created the need for parking and access for automobiles onto most commercial sites. Lighting in required parking areas must be designed to have a minimal impact on the surrounding environment.

(F) Traffic. Developments which generate significant increases in traffic will include an analysis of the proposed development's impact on the current and future transportation system, and methods to control traffic.

1. Major internal circulation roadways that provide access from the public right-of-way or between buildings on the same site must be separated from parking areas using curb and gutter and a combination of any of the following methods:
  - (a) landscaping;
  - (b) pedestrian walkways; or
  - (c) lighting.
2. Concrete "deadmen" wheel stoppers are required for parking spaces adjacent to a pedestrian walkway or amenity. ~~(c) Cross access for automobiles and pedestrians to future and existing commercial developments is required during initial site planning and construction.~~
3. Cross access for automobiles and pedestrians to future and existing commercial developments is required during initial site planning and construction.
4. The design and location of loading areas and dock facilities must minimize the interaction between trucks and visitor's automobiles. Access to loading and delivery areas must be separated from parking areas.
5. The design and location of loading facilities must take into consideration the specific dimensions required for the maneuvering of large trucks and trailers into and out of loading position at docks or in stalls and driveways.

- (G) Pedestrians. Active Transportation Site layout and design must ~~equally~~ address the needs of pedestrians, bicycles, and ~~automobiles~~ other ~~non-motorized~~ forms of transportation. ~~Pedestrian access~~ Access and safety on the site should be ensured for all users.
1. Pedestrian access from the public right-of-way through the site to main entrances is required. These walkways are required to be constructed of dissimilar materials to the roadway or parking area, and provide for clearly defined crossings where there are points of conflict with automobiles.
  2. Dissimilar roadway materials within parking lots are required to be used directly in front of entrances to slow motorists and create a safer environment for pedestrians.
  3. Pedestrian drop-off locations when incorporated within overall circulation patterns are required to be convenient and safe for pedestrians.
  4. An approved bike rack shall be installed near the entrance(s) of each building or unit as applicable.
- (H) Security. Security of the site is required to be addressed in site design. The developer is required to submit documents that demonstrate the security measures of the site design in relation to private, semi-public and public areas, by utilizing natural surveillance, access control and proper maintenance.

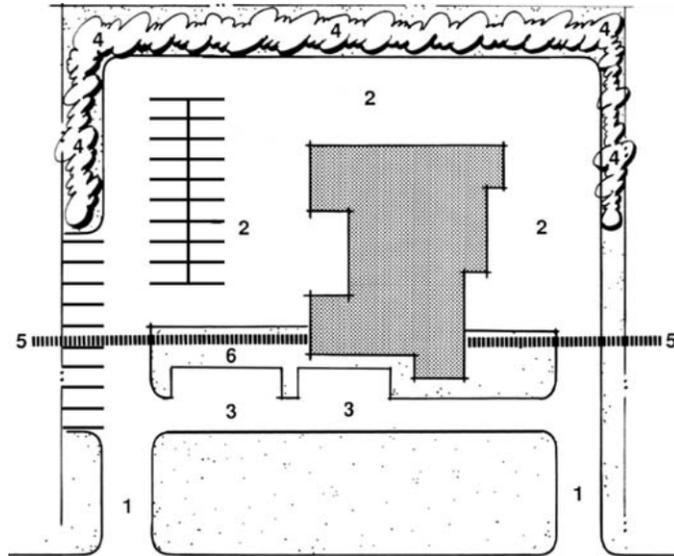
#### 10.28.200 Industrial Developments

The design ~~guidelines~~ standards in this Part are applicable to all new industrial, and warehouse ~~developments~~ buildings in the City as well as additions to and exterior remodeling of existing buildings where the improvements are visible from a public right-of-way.

#### 10.28.210 Industrial Site Planning

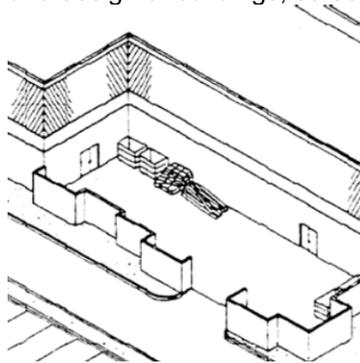
Site planning considers how the various components of a development (e.g., buildings, circulation, parking, open space, etc.) relate to adjacent streets and existing development, and how the various components relate to each other within the development site.

- (A) Building and Facilities Location. The main elements of a well-designed industrial site include:
1. controlled site access with appropriate maneuvering areas for trucks separated from general vehicle circulation (1);
  2. employee parking areas located apart from loading and service areas. (2);
  3. convenient public access and short-term visitor parking at the front of the building (3);
  4. screening of storage, work areas, and mechanical equipment and buffering of adjacent land uses (4);
  5. emphasis on a well-designed main building entry and landscaping (5)



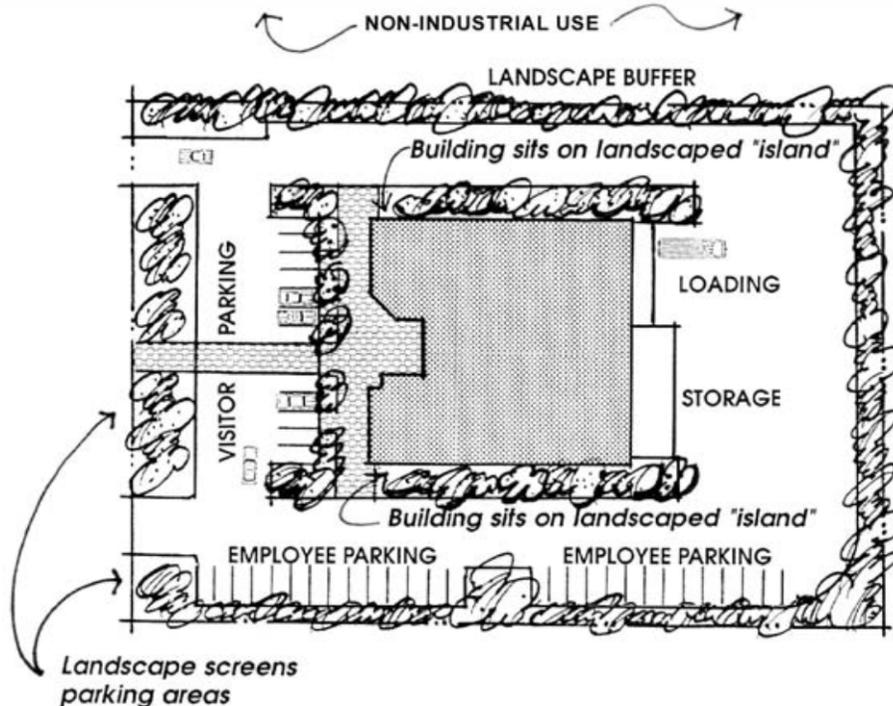
*Example of appropriate industrial site layout*

6. Site elements, such as buildings, parking, driveways, and out-door activities should be arranged to emphasize the more aesthetically pleasing components of the site (e.g., landscaping and superior architectural features) and disguise less attractive elements (e.g., service facilities, outside storage, equipment areas, and refuse enclosures) through proper placement and design of buildings, screen walls, and landscaping.



*Outdoor storage areas-screened from view*

7. Industrial and warehouse development must be screened and buffered from any adjacent uses that are less intensive in compliance with the Code. Intensified landscaping, increased setbacks, and appropriate building location will be utilized as a means of providing adequate separation between land uses of varied intensity.



8. Noise generating functions must be located in a way that minimizes any impact on adjacent non-industrial properties. Sound attenuation walls should be used where appropriate to mitigate/reduce noise.
9. The number of site access points are to be located at distances from street intersections that will minimize points of potential vehicle conflict, especially between automobiles and trucks.
10. Primary entry drives for automobiles, especially visitors, must be enhanced with: ornamental landscaping, low-level decorative walls, monument-type signs, or decorative paving to emphasize site access locations.

(B) Vehicular Circulation

1. Site access and internal circulation must promote safety, efficiency, convenience, and minimize conflict between trucks and other vehicles. Appropriate maneuvering and stacking areas for trucks should be a primary consideration in the overall design of the circulation system.
2. Uses where trucks are anticipated, such as distribution centers, should be planned to accommodate safety and maximize visibility for both truck maneuvering and other traffic.
3. The design and location of loading areas and dock facilities must minimize the interaction between trucks and other automobiles. Access to loading and delivery areas must be separated from parking areas.
4. The design and location of loading facilities must take into consideration the specific dimensions required for the maneuvering of large trucks and trailers into and out of loading position at docks or in stalls and driveways.

(C) Parking Location

1. Parking lots are not to be the dominant visual element at the front of the site. Expansive paved areas located between the street and the building are prohibited.
2. Large parking areas (over 100 spaces) must be divided into smaller multiple lots and provided with trees located throughout the parking area to reduce the visual impact.
3. Visitor parking spaces should be located to produce the shortest route of travel to a building entrance.
4. Pedestrian walkways must provide safe, convenient, and well-defined access between parking areas and the public sidewalk and the main public access to the building.
5. Pedestrian circulation should be clearly delineated and separate from vehicle circulation. The use of landscaping, walkways, or decorative paving to delineate pedestrian circulation must be used.

**10.28.220 Industrial Architecture**

The architectural design of a structure must consider many variables, from the functional use of the building, to its aesthetic design, to its “fit” within the context of existing development. The following **guidelines standards** help buildings achieve the appropriate level of design detail on all facades, avoid blank/uninteresting facades, and provide for the proper screening of equipment and refuse areas.

(A) Architectural Form and Detail

1. If adjacent to a residential zoning district, in addition to the buffer requirements of this code, additional building setbacks of ten feet (10') must be provided adjacent to the residential use to reduce the visual impact of ~~large-scale~~ industrial buildings.
2. The mass and scale of large, box-like industrial buildings are to be reduced through the incorporation of varying building heights and setbacks along the front and street sides **of** building façades.
3. Front and street sides **of** facades of large buildings visible from a public street must include: architectural features such as reveals, windows and openings, changes in color, texture, or material to add interest to the building elevation and reduce its visual mass.
4. Primary building entries must be readily identifiable and well defined through the use of projections, recesses, columns, roof structures, or other design elements.

(B) Color and Materials

1. A comprehensive material and color scheme must be developed for each site. Material and color variations in multi-building complexes must be complementary and compatible among buildings.

**(a) All projects are required to submit a sample board containing physical samples of all exterior surface materials, including roofing materials, in all the colors they will be used. Photos alone are not sufficient.**

2. Large expanses of ~~smooth~~ similar material (e.g., concrete) must be broken up with reveals and/or changes in texture and color.
3. Bright, contrasting colors should be used for small areas of building accents only.
4. Design and colors of wall signs must be compatible with the main buildings on the site.
5. Materials, design, and colors of monument signs must be compatible with the main buildings on the site.

(C) Accessory Buildings

1. The design of accessory buildings (e.g., security kiosks, maintenance buildings, and outdoor equipment enclosures) must be incorporated into and be compatible with the overall design of the project and the main buildings on the site.
2. Temporary buildings are not to be located where they will be visible from adjoining public streets.
3. Modular buildings must be skirted with material and color that is compatible with the modular unit and the main buildings on the site.

**10.28.230 Industrial Landscape Design**

Landscaping has a variety of functions, including softening the hard edges of development, screening unattractive views, buffering less intensive uses, providing shade, and increasing the overall aesthetic appeal of a project.

(A) Landscape Design

1. Landscape design must follow an overall concept and link various site components together.
2. Landscaped areas incorporate a three-tiered planting system: 1) grasses, ground covers, or flowers 2) shrubs or vines, and 3) trees.
3. The use of a variety of trees, especially in parking areas and pedestrian open space areas, is required.
4. More intense landscaping and special landscape features are to be provided at major focal points, such as entries and pedestrian gathering areas.
5. The front, public portions of buildings must be separated from parking areas by landscaping and pedestrian walkways.

(B) Walls and Fences

1. The colors, materials, and appearance of walls and fences, including walls for screening purposes must be compatible with the overall design character/style of the development.
2. Landscaping must be used in combination with walls and fences to visually soften blank surfaces.

3. When security fencing is required adjacent to streets, it must consist of wrought iron, tubular steel, or similar material supported by masonry piers.

(C) Outdoor Lighting

1. The design and location of outdoor lighting fixtures must preclude direct glare onto adjoining property and streets in compliance with the Development Code. Illumination devices must be installed, directed, and shielded to confine light rays within the property.
2. Outdoor lighting (e.g., location, height, and number) must be designed to foster security. Site and building entries must have enhanced illumination to increase visibility and safety.

(D) Refuse Storage and Utility Equipment

1. Refuse storage areas should be located at the rear of the development and screened from public view in compliance with the Development Code.
2. If refuse storage areas, fuel tanks, generators, and fire check safety valves cannot be located out of public view, the design of refuse storage areas should incorporate architectural screening elements and landscaping compatible with the design of buildings and landscaping on the site.



**SYRACUSE**  
EST. CITY 1935



# DESIGN STANDARDS EXAMPLE GUIDE

Commercial, Professional Office, Multifamily Residential, and Public Facility Developments

# Design Standards Example Guide

## Purpose

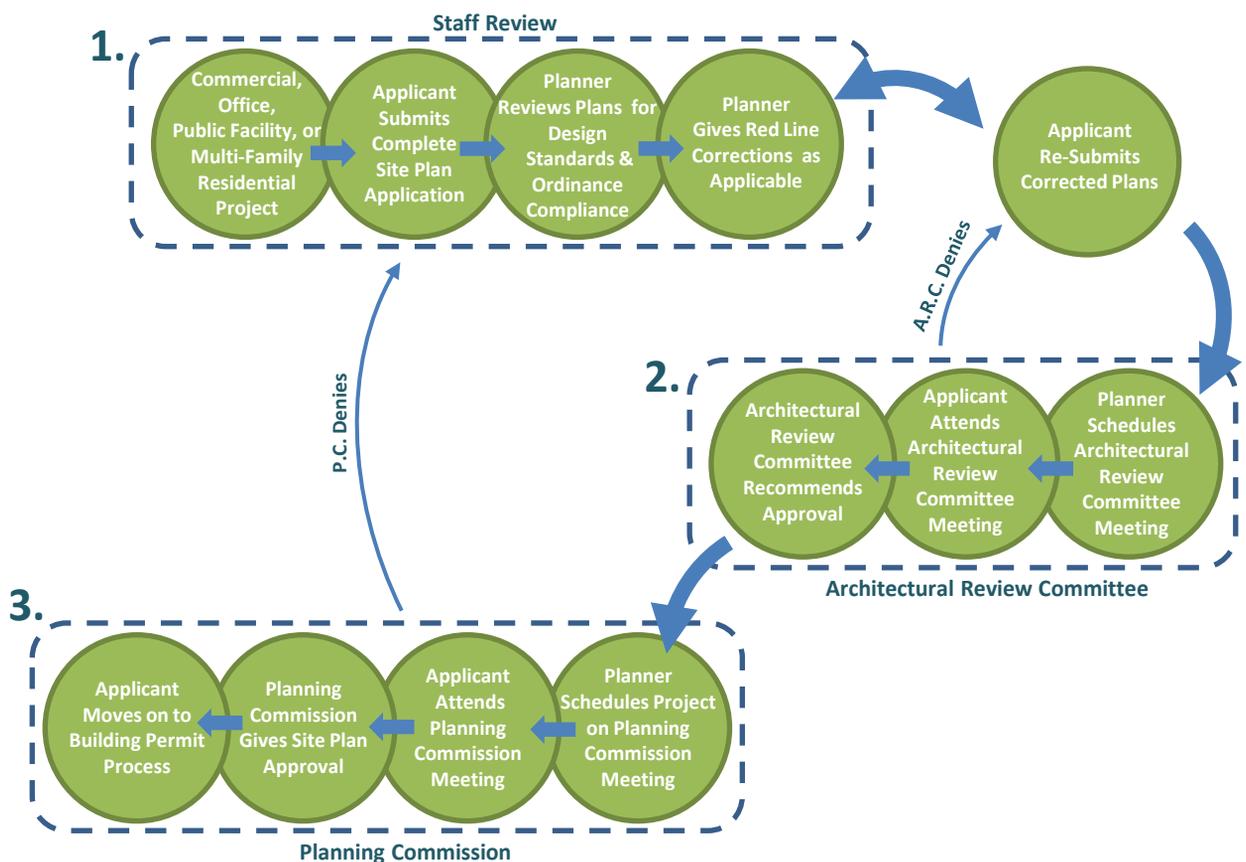
This Guide offers examples to clarify and explain the architectural design standards found in Title 10, Chapter 28 of the Syracuse City Land Use Ordinance. It serves to graphically represent the design standards and are used as a reference tool in order to better illustrate the application of the standards. The purpose of the standards are to improve the quality, compatibility of development, and permanence in design found in Syracuse City. A copy of this manual is available at the Community Development Department located at 1979 W 1900 S Syracuse UT, 84075.

## Applicability

The Design standards referenced in this manual are required in addition to other standards set forth in the Syracuse City Land Use Ordinance. They are applicable with regard to building and site design. The implementation of these standards ensures that the goals and values of the community are reflected in each commercial, professional office, public facility, and multi-family residential development.

## Process

Please refer to the following flow chart for guidance as to the City's process for site plan approval.



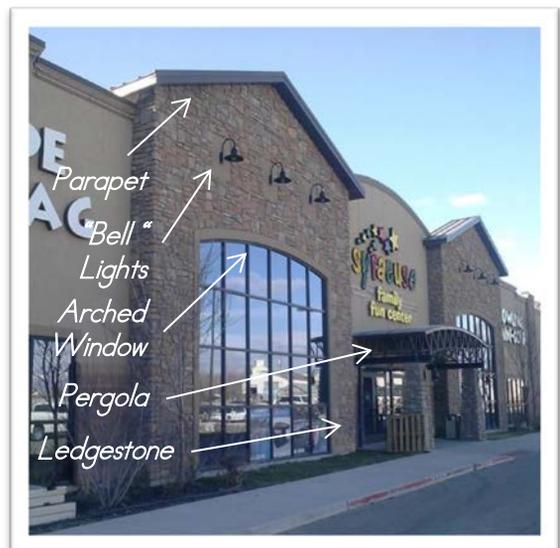
# Design Standards Example Guide

## Building Design

**(A) Context:** New buildings must be designed to include common features and materials that are used on existing Syracuse buildings. These features create a sense of place and character for the City. However, avoid copying too closely and use imaginative design. Please review the following examples of features and materials commonly used.

### List of Contextual Features and Materials:

- Towers – Hipped or Cross Gabled Roofs
- Segmental Style Arched Entries & Windows
- Circular Accent Windows
- Gabled (Pitched) Roof
- Red Brick/Block
- LedgeStone Style Stone
- “Bell” Style Light Fixtures
- Awnings/ Pergolas
- Parapets (False Fronts)
- Metal Channel Roofing



“The Rush” Fun Center - 1806 South 2000 West

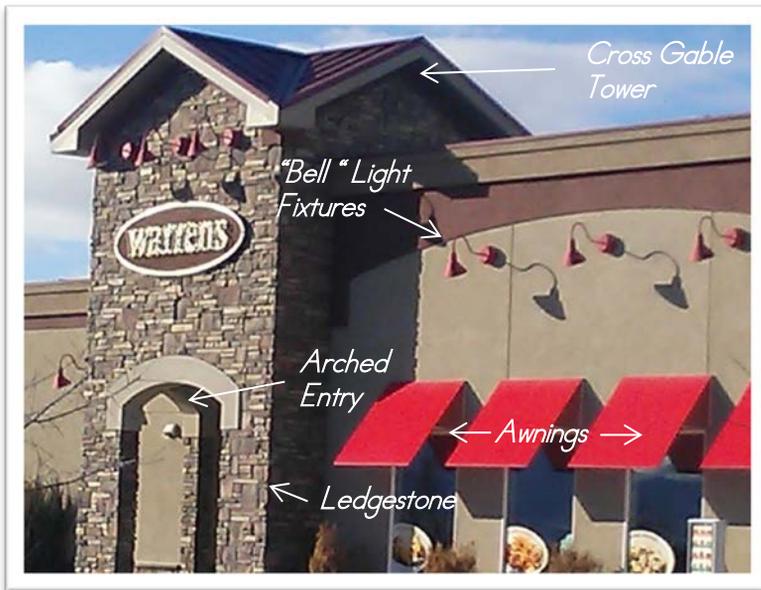


Syracuse Library - 1875 South 2000 West

# Design Standards Example Guide

## Building Design

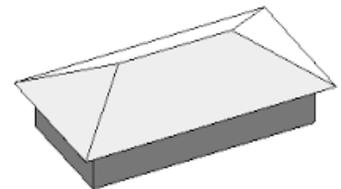
(A) Context: (Continued)



Warrens - 1778 S 1000 W



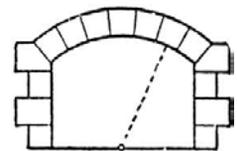
Example of Ledgestone Pattern



Example of Hipped Roof



Carl's Jr. - 2118 W. Antelope Dr.



Example of Segmental Arch



Example of "Bell" Light Fixture

# Design Standards Example Guide

## 10.28.110 Building Design

**(B) Entrances:** All building entrances are required to be clearly articulated to indicate a transition from the exterior to the interior of the building. Every main entrance is required to have a special emphasis when compared to the other portions of the building.



# Design Standards Example Guide

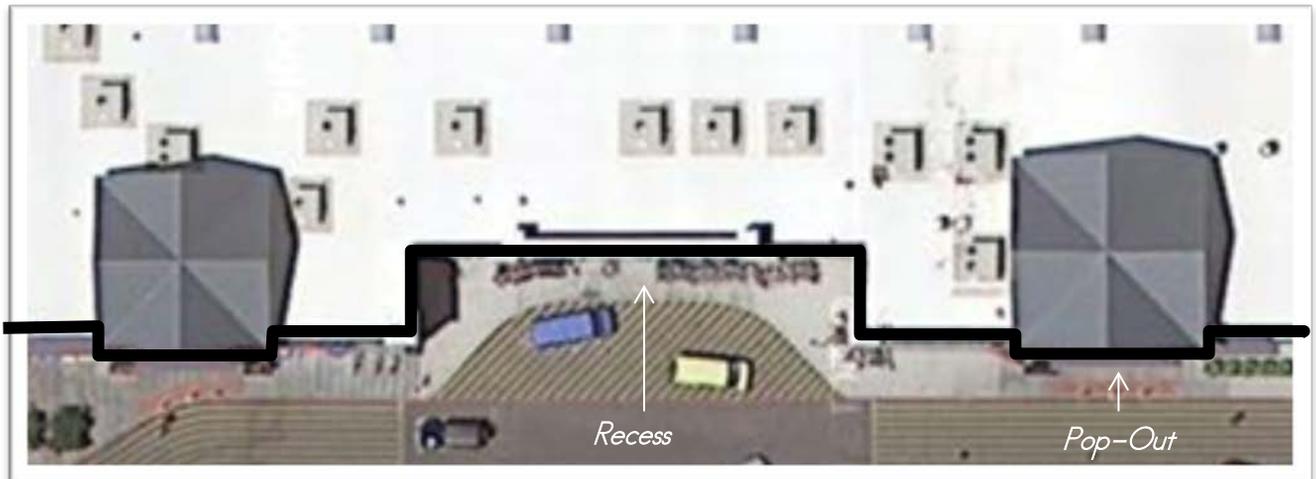
## 10.28.110 Building Design

(C) **Façade Articulation:** Buildings designed with completely flat façades and monotone color schemes are not permitted. All buildings are required to have horizontal and vertical façade variations such as pop-outs, bays, recesses, arches, banding, columns, or similar features. Such features are required at least every 30 feet along all exterior wall planes.

Front Elevation



Plan View



# Design Standards Example Guide

## 10.28.110 Building Design

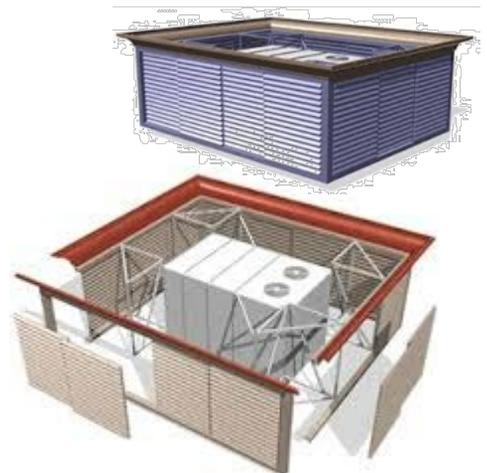
**(D) Height and Roofline:** Varied roofline elevations are required in order to add architectural interest and avoid the appearance or sense of long, monotonous roofline expanses. A variation such as a parapet or tower is required every 50 feet of roofline. Also, mechanical equipment mounted on the roof must be screened and the back of parapets must match the color and materials of the building.



Good Example - 1588 South 2000 West



Avoid Monotonous Rooflines



Proper Mechanical Screening

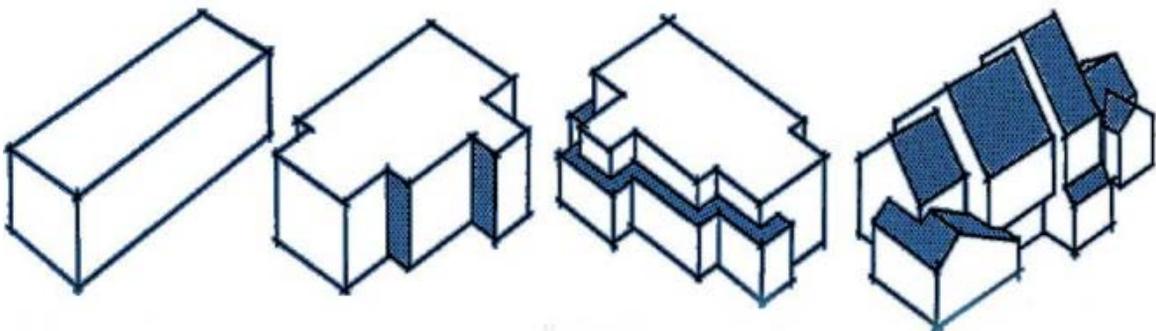
# Design Standards Example Guide

## 10.28.110 Building Design

**(E) Massing:** Proper massing reduces the impact of the massive bulk created by large buildings that may not otherwise relate in scale to surrounding development. Vertical articulation, horizontal articulation, and multi-planned roof or awnings must be used in designs to mitigate the impact on surrounding development and the overall landscape. The examples below are near the same density of units per acre and building height, but the top example's massing reduces the impact and bulk of the building more effectively than the bottom example.



Good Example – 910 W. 1920 S.



Undesirable  
Massing

Vertical  
Articulation

Horizontal  
Articulation

Multi-Planned Roof  
Reduces Bulk



# Design Standards Example Guide

## 10.28.110 Building Design

(G) **Development Design Pattern Book:** The developer is required to provide a development design pattern book for developments including more than one structure. A design pattern book should include:

1. Written descriptions with graphics explaining how the development complements the unique characteristics of the property.
2. Written descriptions with graphic illustrations/photos explaining the theme and physical form of the architectural design.
3. Written descriptions with graphic illustrations/photos describing the proposed open spaces, pedestrian pathways, and other amenities

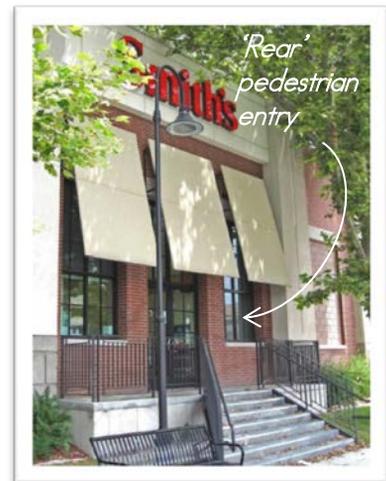


Example Design Pattern Book

# Design Standards Example Guide

## 10.28.110 Building Design

**(H) Pedestrians:** All buildings will be designed with an integral focus on encouraging pedestrian activity and social interaction. Additionally, buildings that contain more than one story or that are above 20 feet in height are required to provide a clearly articulated and more detailed base that relates to pedestrians.



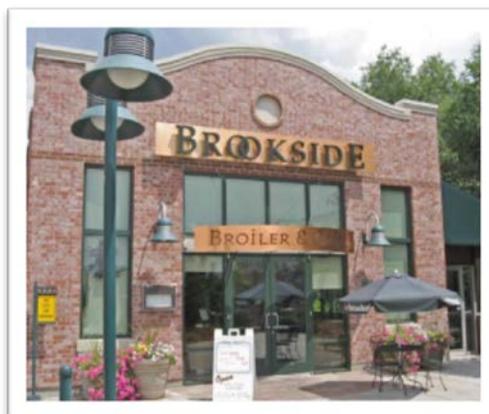
Sugarhouse Utah Smiths

## 10.28.110 Building Design

**(I) Signs:** Signs located on any building façade are required to be compatible with the building's overall design. As an integral design element, signs are required to be compatible with the style of the buildings in terms of location, scale, color, and lettering.

1. The locations for signs on a building's façade will be planned for as part of the building's overall design.

2. Signs located on façades should integrate similar or complimentary materials as the building.



Complimentary Sign Materials



Sign Locations Not Incorporated Into Architecture

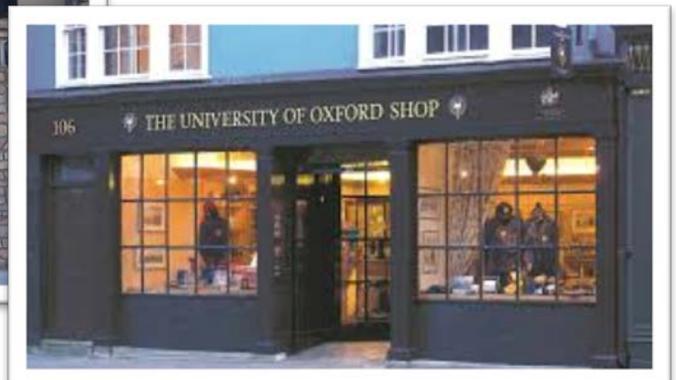
# Design Standards Example Guide

## 10.28.110 Building Design

**(J) Windows:** Windows are key to the overall design of a building and the relationship between the exterior and interior. The majority of windows are required to relate to the scale of a person.



*Mirrored glass not as welcoming*



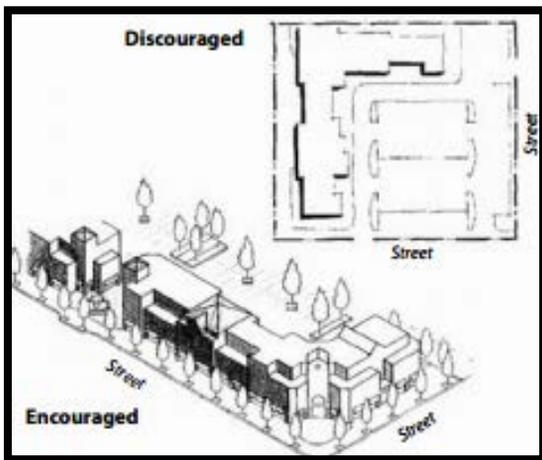
*Transparent windows draw customers in*

# Design Standards Example Guide

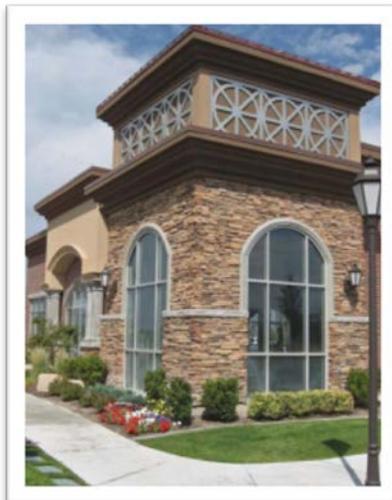
## 10.28.120 Site Design

**(A) Building Placement:** Building placement is integral to the site design and the overall effect any development has on surrounding properties. Building placement is required to comply with the following:

1. All buildings must be oriented with the main or similar façade facing a principal street to which it has frontage.
2. Buildings located on corner lots are required to orient main façades to each street and give equal treatment to each.



3. Buildings located on corner lots should include a prominent architectural feature of greater height than the rest of the roof, or emphasis at the corner where the two public streets meet.

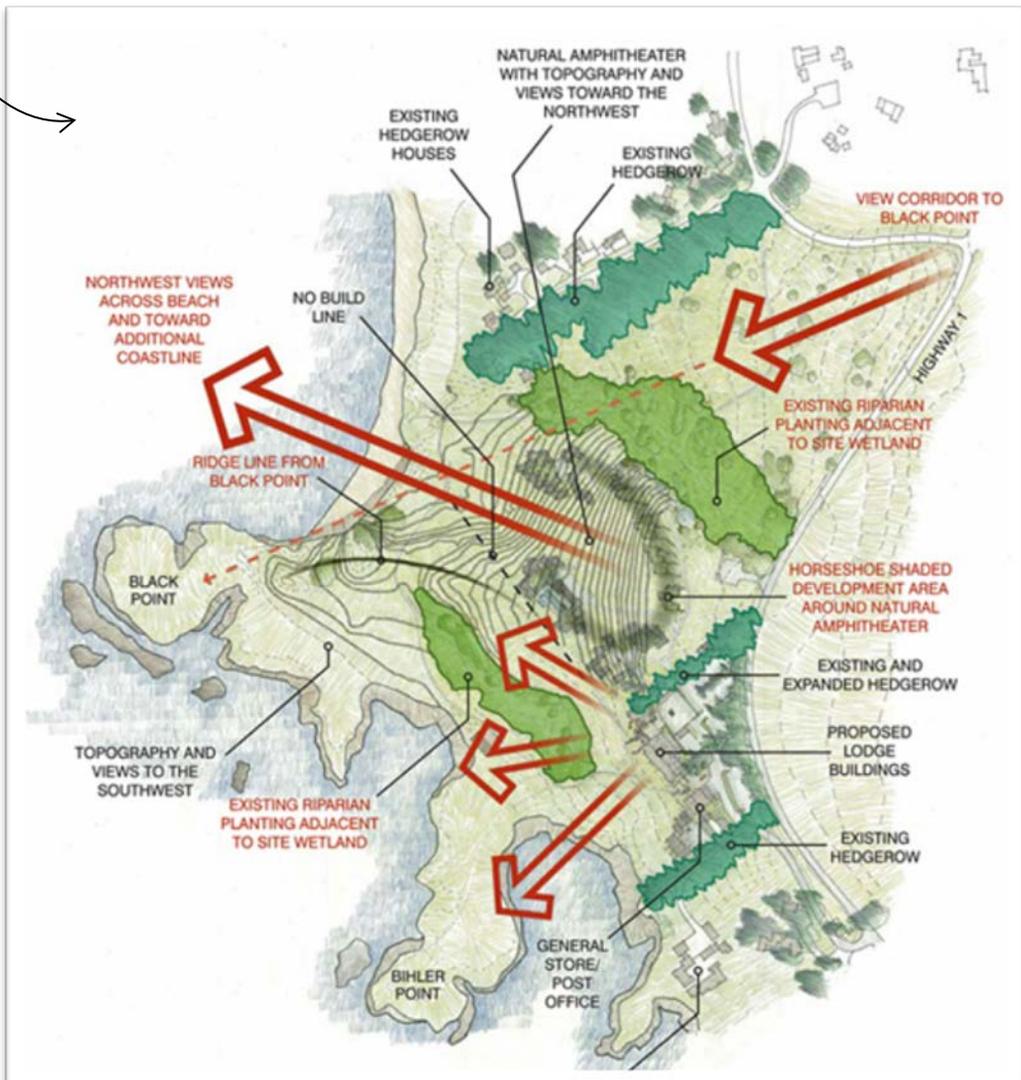


# Design Standards Example Guide

## 10.28.120 Site Design

**(B) Context:** New developments are required to match or compliment surrounding developments and landscapes in order to create a site which relates to its surroundings and adds positively to the overall environment in the site area.

- *Natural features*
- *Wetlands*
- *Historic Structures*



Site Analysis Example



# Design Standards Example Guide

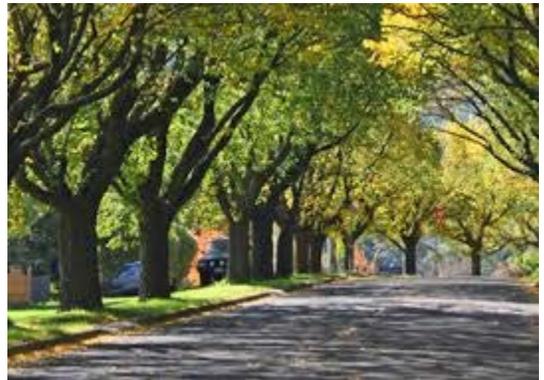
## 10.28.120 Site Design

**(D) Landscaping:** Landscaping is required as a tool to enhance and beautify the site, and the building's architecture and design. Street trees and landscape increase property values.

1. A detailed landscape plan designed by a licensed landscape architect is required.
2. Street trees shall be planted every 30 feet along the public right of way.
3. Native plant species will be used with water efficient irrigation systems.
4. Outdoor amenities such as patios, plazas, water features and outdoor seating areas are encouraged.
5. Existing trees and landscape features should be preserved and incorporated into landscape plans.
6. Landscaping around the base of the building is required



Use Native Plants



Plant Street Trees Every 30 Feet.

Hire a Licensed Landscape Architect →



Landscape the base of buildings and near entrances



Incorporate Outdoor Patios & Seating Areas

# Design Standards Example Guide

## 10.28.120 Site Design

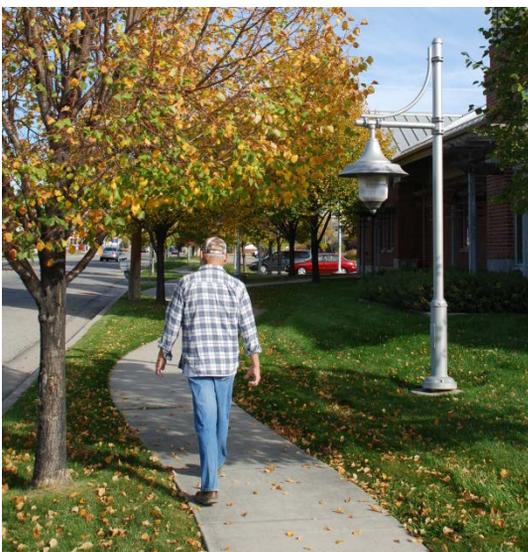
(E) **Lighting:** Carefully planned lighting schemes can create safe environments for pedestrians and motorists. Lighting is an integral design element which adds to the overall site plan and building design.



Coordinated light fixtures on signage and building adds character



Fixtures complement building architecture



Pedestrian scale fixtures increases safety



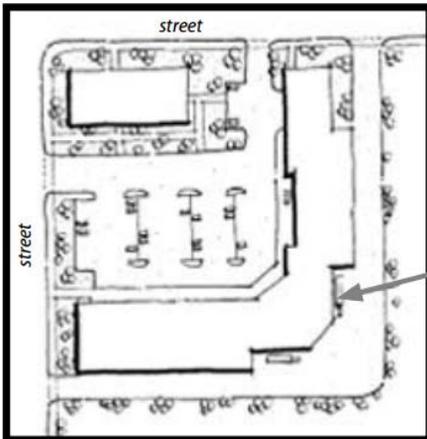
Down facing fixtures minimize light pollution

# Design Standards Example Guide

## 10.28.120 Site Design

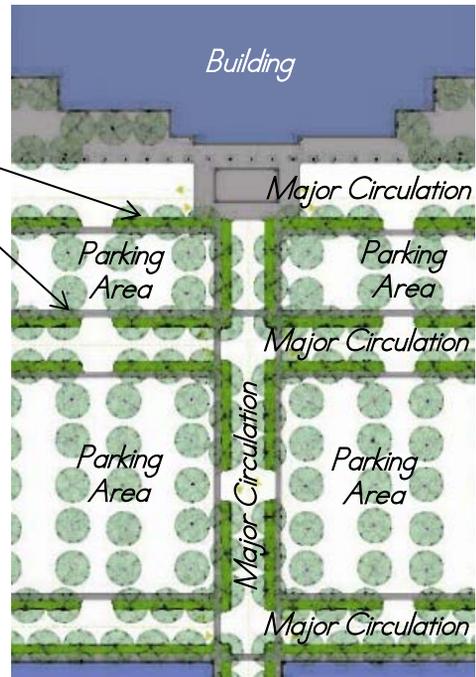
**(F) Traffic:** Developments which generate significant increases in traffic will include an analysis of the proposed development's impact on the current and future transportation system, and methods to control traffic.

- Major internal circulation roadways must be separated from parking areas using curb and gutter, landscaping, pedestrian walkways; or lighting

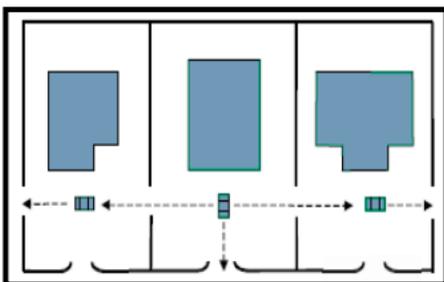


Landscaping  
Sidewalk

Loading and service areas should be located away from street edge and public circulation patterns.



Cross-access for automobiles and pedestrians is required



Automobile Cross-Access



Pedestrian Cross-Access

# Design Standards Example Guide

## 10.28.120 Site Design

**(G) Active Transportation:** Site layout and design must address the needs of pedestrians, bicycles, and other forms of transportation. Access and safety on the site should be ensured for all users.



Bike Rack/Bench



Wheelchair Access



Strollers

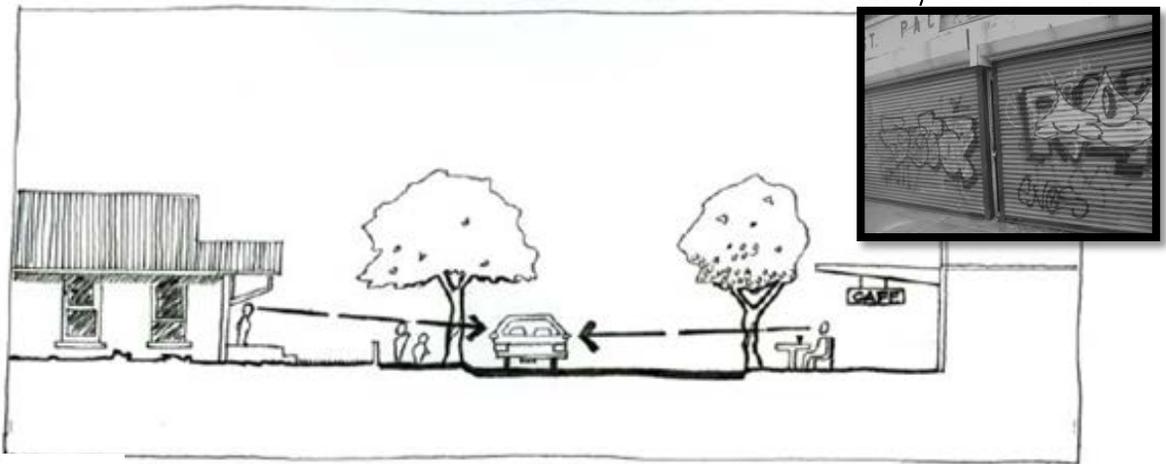
# Design Standards Example Guide

## 10.28.120 Site Design

**(H) Security:** Security of the site is required to be addressed in site design. The developer is required to submit documents that demonstrate the security measures of the site design in relation to private, semi-public and public areas, by utilizing natural surveillance, access control and proper maintenance.

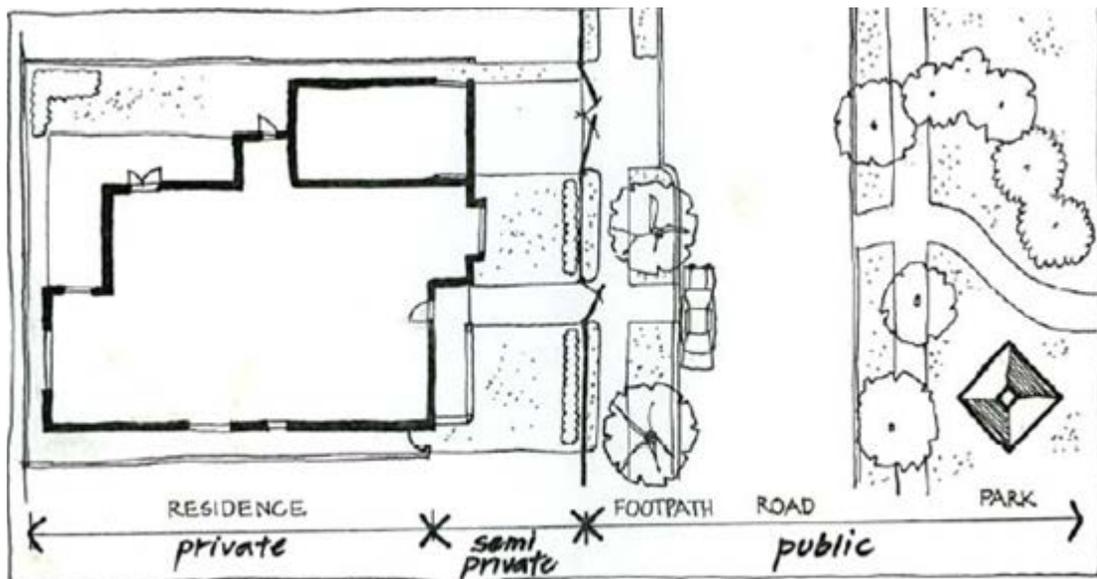
*Natural Surveillance:*

*Proper Maintenance:*



*A number of observers have clear sight lines to the streetscape allowing for natural surveillance*

*Access Control/ Public Private Delineation*



*Clearly marking the distinction between private, semi-private and public space allows for territorial reinforcement*

# Design Standards Example Guide

## Resources

**Architectural Review Committee Design Standards:** Syracuse City Ordinance Title 10 Chapter 28

**Allowed Street Trees:** Syracuse City Ordinance Title 10 Chapter 30 Section 7

**Land Use Ordinances:** Syracuse City Ordinance Title 10 Chapter 6

**Parking Requirements:** Syracuse City Ordinance Title 10 Chapter 8

**Site Planning:** Syracuse City Ordinance Title 10 Chapter 4 Section 90

**Crime Prevention Through Environmental Design (CEPTED):**

<http://www.cadca.org/files/CEPTED%20Handbook%20overview.pdf>

**Native Landscape Plants:**

<http://theutahhouse.org/files/uploads/Utah%20House%20Plant%20List%20v2%204p.pdf>