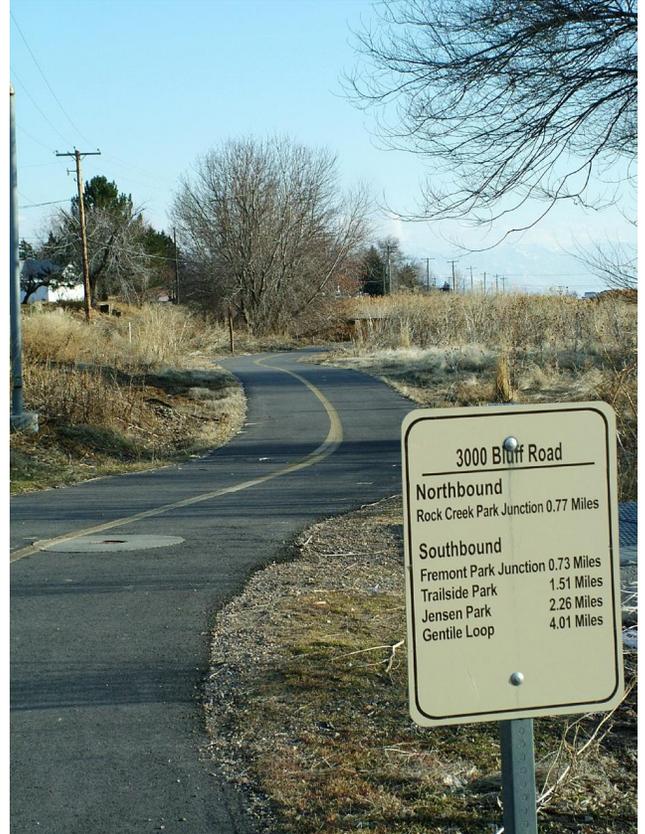




SYRACUSE CITY TRAIL SYSTEM MASTER PLAN



Appendix 1

Adopted by the Syracuse City Council
March 14, 2012
Ordinance No. 12-04



Introduction

The Syracuse City Trail System Master Plan stems from the community's desire to enjoy enhanced outdoor recreation opportunities. Like many communities throughout the U.S., Syracuse City is experiencing a resurgence in bicycling and walking.

The bicycle is a low-cost, quiet, non-polluting, energy efficient, versatile, healthy and fun means of transportation. Bicycles also offer a low-cost mobility option, especially to the young. Walking is the oldest and most basic form of human transportation. It is clean, requires little infrastructure, and is integral to the health of individuals and communities. People who walk know their neighbors and their neighborhood.

The Trail System Master Plan is a long-term guide to future planning, design and implementation of a citywide system of trails to be utilized for commuter travel, health and fitness, and recreational purposes throughout Syracuse. A key element to the plan is interconnecting various neighborhoods within Syracuse as well as regional trail systems being developed by other entities. City parks, neighborhood schools, and future development provide great opportunities for interconnecting pathways.

Community Overview

Syracuse City is a trail-friendly community, due to its natural characteristics. These characteristics include: a moderate climate, relatively flat terrain, low traffic volumes, as well as attracting those who are interested in a healthy lifestyle, a clean environment, and livable and safe neighborhoods. Its beauty and gateway to Antelope Island and the Great Salt Lake Shorelands has long attracted regional bicyclists and worldwide tourists.

Vision

To maintain the "Syracuse City Trail System Master Plan" which will enhance the quality of life by: developing a sense of place, increase outdoor recreation opportunities, preserve open space, enhance the beauty of our community, promote healthy lifestyles, and foster economic development.



Goals

Expand the trail facilities which are conveniently located, safe, and designed to be adaptable to changes in the population, and provide beauty and functional efficiency to complement both the City's natural environment and the needs of its schools and citizens.

Maintain the continuity of the trail master plan network so that it may remain cohesive with the current and future transportation network.

Identify resources to support the improvement, maintenance and operation of existing trails, and the planning, acquisition and development of future trails.

Definitions

Bike Lane:

A designated lane independently delineated for bicycle travel on a public street.

Development:

Improvement of land in any zone for any purpose by adding, modifying, or enhancing structures and/or supporting infrastructure.

Equestrian Trail:

An independent trail which has a non-paved surface,

such as a natural, native, or loose granular material for use of horse riding.

Shared Lane:

A designated travel lane inside a public right-of-way that is open to both bicycle travel and vehicular use. This type of lane is typically utilized on existing streets where street widths are already established and prohibit the use of bike lanes. These lanes are marked with a standard pavement marking designated by Manual of Uniform Traffic Control Devices (MUTCD).

Shared Use Trail:

An improved path located inside a public right-of-way that is physically independent from motorized vehicular traffic by an open space or barrier and is utilized by cyclists, joggers, pedestrians, scooters, skaters, strollers, wheelchairs (motorized and non-motorized), and other devices compatible with pedestrian travel.

Trail Access Point:

A designated point of access to the trail system, which provides adequate off-street vehicular parking at desirable locations, that are spread throughout the trail network.

Trail:

A path defined on the map attached to the master plan which could be a bike lane, equestrian trail, and/or shared use trail.

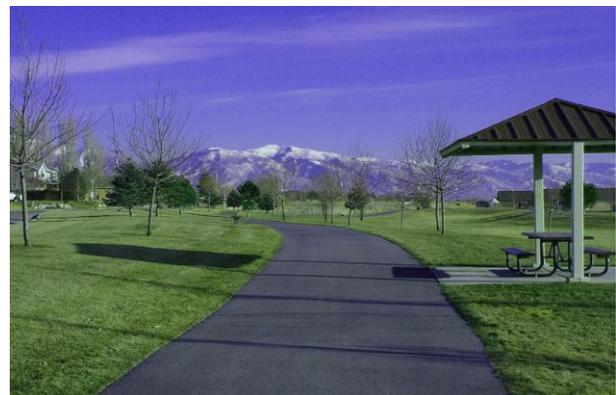


Policies

1. Work with new development to incorporate trails where recommended on the attached map, utilizing incentives that may be established by

ordinance.

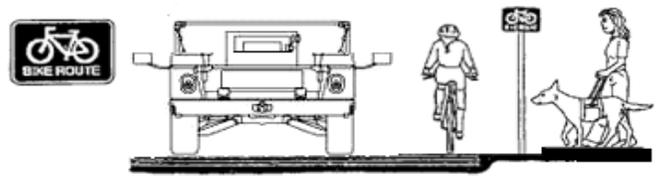
2. Work closely with local organizations to facilitate the creation, maintenance, and joint use of trails, access points, amenities and connections.
3. Emphasize safety as an essential component of the trail system utilizing current local, State, and federally accepted design standards, as well as incorporating safety measures such as: adequate lighting, trail signs and markings, ongoing maintenance, citizen patrols and similar other measures to ensure the safety of trail users.
4. Develop a connected trail system that will link city neighborhoods, parks, and trails developed/planned by adjacent entities.
5. Coordinate with adjacent communities, local county, regional agencies, and State agencies as necessary when portions of the Trail System Master Plan is improved, expanded, or modified.
6. Encourage the city to develop an *Adopt-a-Trails* program that governmental, volunteer and private organizations can participate in to facilitate the maintenance of current and future trails.
7. Encourage trail system enhancements such as benches, historic/cultural markers, gateways, exercise stations, picnic areas, rest areas, restrooms and/or landscaping as appropriate to make the trails more interesting, functional, and enjoyable.



8. Designate trail access points with off-street

parking facilities to provide easy access to the trail network for all citizens regardless of physical ability.

9. Encourage the city to provide consistent trail system management signage for the benefit and safety of all users.
10. Update the trails system plan as necessary to assure that current issues are addressed and to coordinate the plan with the efforts of other governmental agencies.



Class IV- Equestrian Trail



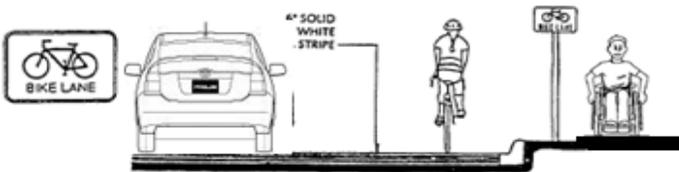
Infrastructure

This plan uses four types of classifications for trails. These are identified as follows:

Class I - Shared Use Trail



Class II - Bike Lane



Class III - Shared Lane



Implementation

In order to implement the trails as established in the "Syracuse City Trail System Master Plan," the city should follow the trails planning process for each trail segment:

1. Analyze the ownership of the various trail segments identified on the Plan
 - Prioritize the right-of-way acquisition needs based on the potential for development to occur, land costs and potential usage
 - Formulate a set of design guidelines for each trail type
 - Estimate acquisition and construction costs

- Identify funding sources
2. Develop a trail Signing Program that includes:
 - Trail Logo Sign(s)
 - Bike Route and Bike Lane signs
 - Trail "Links" through subdivisions and where appropriate
 3. Provide trail system management signing where necessary advising users about:
 - Overtaking protocol (proper passing)
 - Slower traffic staying to the right
 - Leash requirements and dog etiquette
 - Any applicable enforcement codes
 4. Develop a trail section which considers:
 - Paved section – 10 feet wide
 - Separate unpaved surface on one or both sides of the pathway for runners and walkers, and equestrian users
 - Centerline striping when volumes are high
 - Security lighting where necessary
 5. Publicize pathways in order to encourage community use by:
 - Posting the route on the City's web site
 - Conducting a pathway logo contest
 - Naming pathways for donors that contribute significant land, materials, etc. that add to the development of the pathway system
 6. Incorporate bike lanes on existing streets following the intended routes shown on the map (or considering alternate routes, if necessary in order to create continuity in the overall trail system). Bike lane installations should be considered in situations where:
 - Interconnecting existing bike lanes are beneficial
 - Street resurfacing projects are performed
 - Improvements for public safety are beneficial
 7. As non- motorized trails are expanded, consideration will be given to interconnect points of interest, such as:
 - Emigrant Trail
 - Scenic vistas
 - Antelope Island
 - Equestrian trails
 - Unique landforms
 - Unique wildlife habitats
 - Town Center
 - Great Salt Lake
 - The Nature Conservancy's, Great Salt Lake

- Shorelands Preserve
- Schools
- Parks
- Historic Markers

8. As opportunities to expand the trail system arise, consideration should be given to connecting new trails to existing trails as well as city amenities and the trail system.



Specific Map Notes

- 1) The Syracuse Emigrant Trail connects with several parks within the City, and continues north into West Point City.
- 2) The Great Salt Lake Shoreline Trail and Trail Access points are integral to the city's trail system, providing connectivity between the Bird Refuge and Antelope Island Causeway.





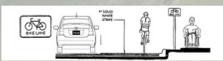
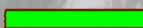


Syracuse City Existing Trails Map

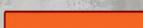
Legend



Class I
Shared Use Trail



Class II
Bike Lane



Trailhead



Restrooms

Map Notes

- 1) The Syracuse Emigrant Trail connects with several parks within the City, and continue North into West Point City.
- 2) Visit the Syracuse City website for an online version of this map.

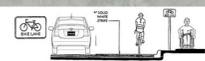


Syracuse City Trails Master Plan

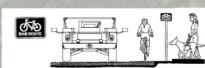
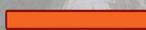
Adopted by City Council on 3/14/2012
Ordinance No. 12-04



Class I
Shared Use Trail



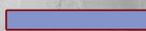
Class II
Bike Lane



Class III
Shared Lane



Class IV
Equestrian



Bike Route (See Map Note 3)



Trailhead



Restrooms

Map Notes

- 1) The Syracuse Emigrant Trail connects with several parks within the City, and continues North into West Point City.
- 2) The Great Salt Lake Shoreline Trail and Trail Access points are integral to the city's trail system, providing connectivity between the Bird Refuge and Antelope Island.
- 3) Note the Bike Routes which run adjacent to the designated Class I Trails along Gentile Street and Bluff Road.
- 4) This document shows the current and proposed trail sections.
- 5) Visit the Syracuse City website for an online version of this map.

